

VILLAGE OF BARRINGTON HILLS

Roads & Bridges Committee

NOTICE OF MEETING



Thursday, May 21, 2015 ~ 3:00 pm
112 Algonquin Road

AGENDA

1. Organizational
 - 1.1 Call to Order
 - 1.2 Roll Call
2. [Vote] Minutes March 24, 2015
3. Public Comments
4. Discussion Items
 - 4.1 2015 Road Program Bids – Bid Opening Scheduled 5/21/15 @ 2:00 PM
 - 4.2 Cuba Road Bridge Update
 - 4.3 2015 Drainage Program
 - 4.3.1 Merri Oaks Road – Construction Update
 - 4.3.2 Steeplechase Road Drainage
 - 4.3.3 Lakeview Lane Drainage
 - 4.4 IDOT & County 2015 Road Construction
 - 4.4.1 2015 Road Construction Map
 - 4.4.2 KDOT - Longmeadow Parkway Pre-Final Plans
 - 4.4.3 [Vote] R & B Committee Position on Res. 06-S*
 - 4.4.4 LCDOT- Rte. 14/Hart Road Project Status
 - 4.4.5 [Vote] R & B Committee Position regarding Feb 25, 2015 letter to Lake County Division of Transportation regarding proposed Sidewalk
 - 4.5 Functional Classification Requests- Ridge Road & Plum Tree Road
 - 4.6 On-street Parking
5. Adjournment

Chairman: Brian Cecola

NOTICE AS POSTED

112 Algonquin Road ~ Barrington Hills, IL 60010-5199 ~ 847.551.3000

VILLAGE OF BARRINGTON HILLS

Roads & Bridges Committee

Meeting Minutes

March 24, 2015



Committee Members Present: Trustee Patty Meroni, Chair
President Martin McLaughlin
Robert Kosin, Director of Administration
Dan Strahan, Village Engineer

Others Present: Mary Naumann, Resident
Pauline Boyle, Resident
Don Whitaker, Resident
Linda Cools, Resident
Brian Cecola, Resident

1. ORGANIZATIONAL: The meeting of the Village of Barrington Hills Roads & Bridges Committee was called to order at 3:00 p.m. by Chairman Meroni.

2.1 APPROVAL OF MINUTES: The minutes of the Roads & Bridges Committee Meeting of February 17, 2015 were approved as written.

3. PUBLIC COMMENTS: Ms. Cools requested information regarding the start date of the Haegers Bend Road project and asked if follow-up correspondence would go out to the residents.

4.1 VARIOUS AGENCY PROJECTS: Mr. Strahan provided an updated regarding the proposed Highland Avenue/Spring Creek Road project being designed by the Village of Algonquin. It was noted that the proposed bike path improvements were located entirely in Algonquin and would not extend into Barrington Hills. In addition, roadway reconstruction and widening from 22' to 28' was planned within the Village of Algonquin, but the scope of work within Barrington Hills was limited to resurfacing of the existing pavement width.

Mr. Strahan reviewed the project background for the proposed intersection improvements at Hart Road and US 14, currently in Phase II design by the Lake County Division of Transportation (LCDOT). LCDOT had received a request from a residence for pedestrian connectivity from a resident on the south side of the road and requested that the Village of Barrington Hills provide a portion of the funding, which the Village declined to participate. The Village then received notice from LCDOT that the Village of Barrington had agreed to fund the local share as well as future maintenance costs for the addition of an 8' bike path on the south side of Hart Road. Trustee Meroni indicate she would follow up with Ken Bosworth. It was noted that typically Lake County DOT would have completed all phases of the engineering, but the Village of Barrington had initially coordinated Phase I to move the project forward.

Mr. Strahan provided an update on the Longmeadow Parkway project, noting Phase II engineering was in progress. Mr. Strahan summarized discussions with Kane County DOT regarding the potential to provide supplemental detention storage within a proposed detention basin near the future Longmeadow/IL 25 intersection. After review it was determined that the Village would not request any further detention volume beyond the 1.2 acre-feet provided. It was noted that pre-final plans were anticipated by the end of March.

4.2 IDOT RESPONSE FAU RECLASSIFICATION REQUEST: Mr. Strahan noted that a response had been received from IDOT regarding the requested FAU classification modifications that had been approved by the McHenry County Council of Mayors in November. The letter noted that Spring Creek Road had been approved to be reclassified as a local street, Merri Oaks would require concurrence from the Cuba Township Highway Department, and the requested modification to Local Street had been denied for Ridge Road and Plum Tree Road. It was noted that Ridge Road and Plum Tree Road could be reclassified from Minor Arterial to Major Collector. Trustee Meroni requested that this revision be requested.

4.3 BARRINGTON HILLS 2015 ROAD PROGRAM – MFT RESOLUTION: Trustee Meroni noted that the 2015 Road Program would utilize both Village and MFT funds. The MFT resolution for \$100,000 would be presented to the Village Board at the March 30th Board meeting for approval.

4.4 CUBA ROAD BRIDGE: Mr. Strahan provided an update noting that the ROW acquisition is in progress, including Cuba Township. It was noted that one of the properties is in the middle of a foreclosure and bankruptcy, which can delay the acquisition process. It was noted that ROW acquisition would need to be completed by April 29th to allow for the June 12th letting.

Mr. Strahan noted the possibility of providing a project website or providing a webcam of the project during construction. Trustee Meroni indicated she would discuss this possibility with the Board.

Mr. Strahan also noted that GHA was requesting a supplement to their original contract due to completion of a number of tasks outside of the original scope. The additional fees would be included in the 80% reimbursement from IDOT, so additional costs to the Village would be \$2,750.00. President McLaughlin requested that the percentage of the project represented by engineering fees be provided.

4.5 2015 DRAINAGE PROGRAM UPDATE: Mr. Strahan noted that construction on Merri Oaks Road was approximately 25% complete. Comcast was onsite adjusting a cable in conflict with the proposed storm sewer, but it was noted that no further work would be taking place due to the seasonal weight restrictions.

An update was provided on floodplain issues encountered during the planning process for lots on Rub of Green Lane. After meeting with the developer, Lake County Stormwater Management Commission, and the homeowners association, it was determined that the Village would complete some basic modeling to confirm that the proposed floodplain elevation was appropriate.

4.6 SEASONAL ISSUES: Trustee Meroni noted she had been contacted by residents on Lakeview Lane to review a recurring condition of standing water in the ditchlines. Mr. Strahan reviewed past correspondence which reviewed this condition. It was noted that regrading of the ditches along Dundee Road and Lakeview Lane was not feasible due to the existing topography. A second option to route a storm sewer directly to Heather Lake was reviewed. Mr. Whitaker noted that a landscaper may be able to construct a small pipe for much less money than what the Village storm sewer may cost. Trustee Meroni noted that different requirements apply to work undertaken by the Village. It was noted that resurfacing operations on Dundee Road were anticipated to begin on May 1.

President McLaughlin asked what the schedules are for resurfacing on Brinker Road and some of the other state and county roads currently in poor condition. Mr. Strahan noted that Cook County has not previously committed to a schedule.

Trustee Meroni noted that the seasonal weight restrictions had been posted as of March 16, 2015. She also noted that the Cuba Township Highway Department had been contacted to begin pothole repairs.

Trustee Meroni noted that the requested quote for single file bicycle signage had not been acted upon in 2014 and requested an updated quote be provided for consideration at the March 30th Board meeting.

5. ADJOURNMENT: The meeting was adjourned at 3:56 PM.



2015 ROAD PROGRAM

VARIOUS CULVERT REPLACEMENTS

VILLAGE OF BARRINGTON HILLS

SECTION # 15-00024-00-RS



STANDARD SYMBOLS

EXISTING	FEATURE	PROPOSED
BM/T&M	BENCHMARK	BM/T&M
CONCRETE R.O.W. MONUMENT		
R.O.W. LINE		
STORM SEWER		
STORM SEWER MANHOLE		
CATCH BASIN		
INLET		
CLEAN OUT		
SANITARY SEWER		
SANITARY FORCEMAIN		
SANITARY SEWER MANHOLE		
WATER VAULT		
VALVE BOX		
WATERMAIN		
FIRE HYDRANT		
GAS MANHOLE		
GAS VALVE		
GAS MAIN		
TELEPHONE LINES		
IBT BOX		
IBT MANHOLE		
ELECTRIC LINES		
ELECTRIC MANHOLE		
UTILITY POLE		
SIGN		
LIGHT STANDARD		
FENCE		
TREE		
SHRUB		
CONTOUR LINE		
SPOT GRADE		
HEADWALL		
FLARED END SECTION		
CULVERT		
SWALE		
PROPERTY PIN		
STORM SEWER STRUCTURE ELEVATION		
SANITARY SEWER STRUCTURE ELEVATION		
WATER MAIN STRUCTURE ELEVATION		

LOCATION MAP



SHEET INDEX

1. TITLE SHEET
2. GENERAL NOTES & DETAILS
3. HAEGERS BEND CULVERT REPLACEMENT - STA. 105+43
4. HAEGERS BEND CULVERT REPLACEMENT - STA. 112+34
5. HAEGERS BEND CULVERT REPLACEMENT - STA. 130+47.50
6. HAEGERS BEND CULVERT REPLACEMENT - STA. 134+46
7. HAEGERS BEND CULVERT REPLACEMENT - STA. 154+23.50
8. HAEGERS BEND CULVERT REPLACEMENT - STA. 166+63.70
9. HAEGERS BEND CULVERT REPLACEMENT - STA. 175+04.70

NOTE: THE INTENT OF THIS PLAN SET IS TO PROVIDE DETAILED INFORMATION FOR THE CULVERT REPLACEMENTS AND DRAINAGE IMPROVEMENTS SHOWN. DETAILS REGARDING PATCHING AND RESURFACING FOR THE 2015 ROAD PROGRAM ARE PROVIDED IN THE PROJECT SPECIFICATIONS BOOK.

VARIOUS CULVERT REPLACEMENT CONTROL POINTS: (SEE PLAN SHEETS)



NOTE: CONSTRUCTION MEANS, METHODS AND JOB SITE SAFETY IS THE SOLE AND EXCLUSIVE RESPONSIBILITY OF THE CONTRACTOR

COORDINATING/PERMITTING AGENCIES:
VILLAGE OF BARRINGTON HILLS
US ARMY CORPS OF ENGINEERS
(PERMIT FOR SHEET 7 ONLY)

PHONE #:
847-551-3000
312-846-5530

PERMIT APPLICATION #/STATUS
APPROVED
UNDER REVIEW

UTILITY AGENCIES:
COMMONWEALTH EDISON CO. 630-437-2129
SBC 847-506-8716
AT&T 630-592-4677
COMCAST 847-625-1194
NICOR GAS 815-754-3339
NATURAL GAS PIPELINE CO. 815-725-1405

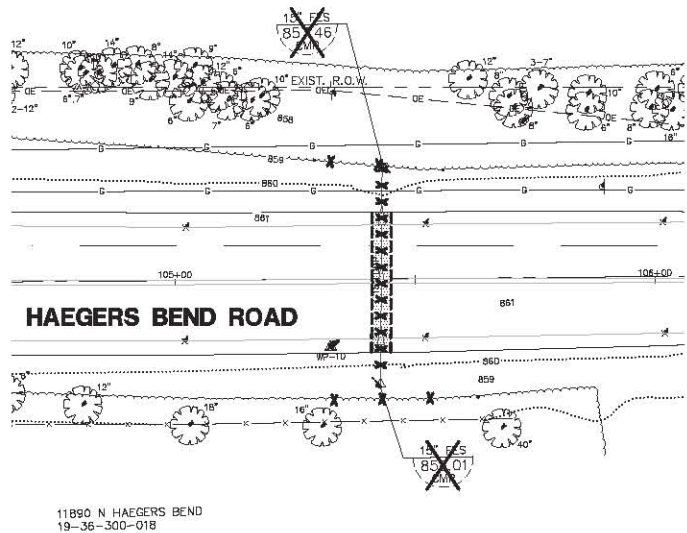
CONTRACTOR SHALL:
NOTIFY THE OFFICES OF THE VILLAGE OF BARRINGTON HILLS ENGINEER THREE (3) WORKING DAYS PRIOR TO COMMENCEMENT OF WORK 847-855-1100
NOTIFY THE VILLAGE OF BARRINGTON HILLS 72 HOURS PRIOR TO THE COMMENCEMENT OF WORK 847-551-3000

GHA GEWALT HAMILTON ASSOCIATES, INC.
625 Forest Edge Drive ■ Vernon Hills, IL 60061
TEL 847.478.9700 ■ FAX 847.478.9701

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TITLE SHEET
2015 ROAD PROGRAM
VARIOUS CULVERT REPLACEMENTS
VILLAGE OF BARRINGTON HILLS, ILLINOIS

FILE 9355.141-PR1.dwg	SHEET NUMBER:
DRAWN BY: GW3	GHA PROJECT #
DATE 03.09.15	9355.141
CHECKED BY: DJS	SCALE
DATE 03.09.15	NTS
NO. BY DATE REVISION	NO. BY DATE REVISION
1	OF 9 SHEETS



Point Table				
Point #	Northing	Easting	Elevation	Description
10	1999287.58	1004780.84	860.92	WP-MAG-10

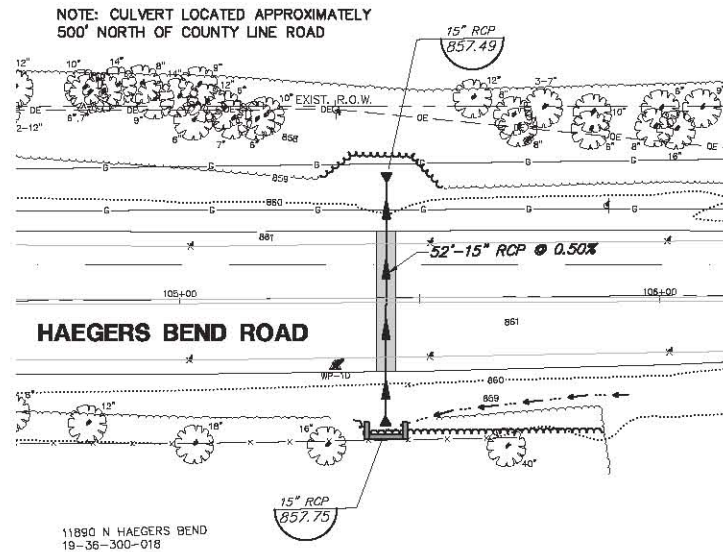
EXISTING/DEMO LEGEND

STORM SEWER REMOVAL	
SAWCUT PAVEMENT (FULL DEPTH)	
DRIVEWAY PAVEMENT REMOVAL (FULL DEPTH)	
TREE OR BRUSH REMOVAL	

DEMOLITION NOTES:

1. AS AN INITIAL ORDER OF OPERATION TRAFFIC CONTROL SIGNS MUST BE IN PLACE PRIOR TO ANY CONSTRUCTION OPERATIONS. A MINIMUM OF ONE LANE OF TRAFFIC MUST REMAIN OPEN AT ALL TIMES. PROVIDE FLAGGERS WHENEVER CONSTRUCTION OPERATIONS LIMIT TRAFFIC FLOW TO ONE LANE.
2. INLET FILTER BASKETS SHALL BE PLACED IN ALL DRAINAGE STRUCTURES PRIOR TO INITIATION OF CONSTRUCTION ACTIVITIES AND REMAIN IN PLACE THROUGHOUT CONSTRUCTION.
3. SAWCUT PAVEMENT (FULL DEPTH) WHERE NOTED AS MARKED IN FIELD BY ENGINEER.
4. PROVIDE BARRICADES ALONG EDGES OF EXCAVATION AT CLOSE OF EACH CONSTRUCTION DAY.
5. WHERE SHOWN EXISTING UTILITY INFORMATION IS APPROXIMATE BASED ON ATLAS INFORMATION PROVIDED BY THE RESPECTIVE UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING JULIE AND FIELD LOCATING/POTHOLING AS NEEDED TO CONFIRM UTILITY DEPTHS PRIOR TO INSTALLATION.

EXISTING CONDITIONS/DEMOLITION



Point Table				
Point #	Northing	Easting	Elevation	Description
10	1999287.58	1004780.84	860.92	WP-MAG-10

PROPOSED LEGEND

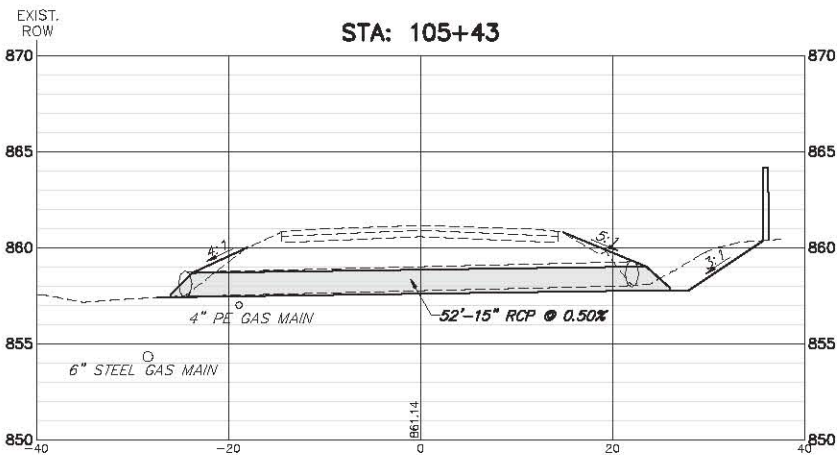
CLASS D PATCHES, 6" (SPECIAL)	
GRADING & SHAPING DITCHES	
COIR ROLLS	
INLET PROTECTION	

NOTES:

1. RESTORE ALL DISTURBED GRASS AREAS WITH 4" TOPSOIL, CLASS 2A SEEDING, & EROSION CONTROL BLANKET.
2. ALL PROPOSED DRAINAGE SWALES SHALL BE BUILT TO MINIMUM 1.0% SLOPE. MINIMUM CROSS SLOPE OF DRAINAGE SWALES SHALL BE 3H:1V.
3. CONTRACTOR SHALL COORDINATE ANY TEMPORARY LOSS OF DRIVEWAY ACCESS WITH RESIDENTS. UNDER NO CIRCUMSTANCES SHALL ANY PROPERTY BE WITHOUT DRIVEWAY ACCESS BEFORE 9:00 AM OR AFTER 4:00 PM.
4. LENGTHS OF CULVERTS SHOWN ARE INCLUSIVE OF LENGTHS OF FLARED END SECTIONS, BUT QUANTITIES WILL BE MEASURED AND PAID FOR SEPARATELY FOR EACH ITEM.
5. WHERE SHOWN, RIGHT-OF-WAY LIMITS AND PROPERTY BOUNDARIES ARE UNPROVEN BUT BASED ON FOUND PROPERTY CORNERS AND OTHER MONUMENTATION. A BOUNDARY SURVEY WAS NOT COMPLETED WITH THIS TOPOGRAPHIC SURVEY.
6. MATERIAL RESULTING FROM TRENCH EXCAVATION MAY NOT BE SIDE CAST INTO WETLANDS OR WATERWAYS.
7. IN WETLAND AREAS, THE TRENCH SHALL BE BACKFILLED WITH TOPSOIL EXCAVATED FROM THE TRENCH IN THE SAME STRATIFICATION IN WHICH IT WAS REMOVED.
8. IN WETLAND AREAS, GRAVEL BEDDING CANNOT BE USED AS BACKFILL MATERIAL IN THE TRENCH.

PROPOSED CONDITIONS

HAEGERS BEND ROAD CROSS SECTION

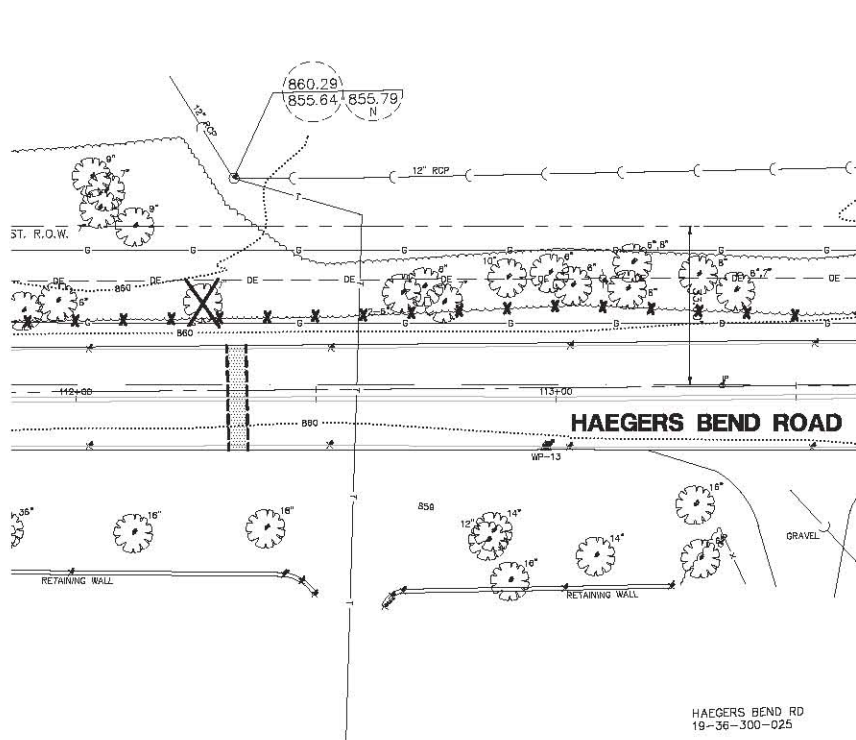


CONSTRUCTION SEQUENCING

1. BRUSH/TREE REMOVAL WHERE NECESSARY (CLEAR & GRUB)
2. REMOVE EXISTING CULVERT.
3. INSTALL PROPOSED ROADWAY CULVERT & REINFORCED CONCRETE END SECTION.
4. COMPLETE GRADING AND SHAPING OF DITCHES & RESTORATION IN THE VICINITY OF THE CULVERT.
5. PERMANENTLY STABILIZE ALL DISTURBED AREAS.

SUMMARY OF DRY UTILITIES

	EXISTING EQUIPMENT	EQUIPMENT TO BE RELOCATED
AT&T CORPORATION	AERIAL CABLE LOCATED ON THE WEST SIDE OF HAEGERS BEND ROAD	NONE
COMCAST		NONE
COMED	OVERHEAD 34 KV WIRE WEST SIDE OF HAEGERS BEND ROAD 1-PHASE OVERHEAD WIRE WEST SIDE OF HAEGERS BEND ROAD	NONE
NICOR GAS	UNDERGROUND GAS MAINS, 4" AND 6" ON THE WEST SIDE OF HAEGERS BEND ROAD	NONE



Point Table				
Point #	Northing	Easting	Elevation	Description
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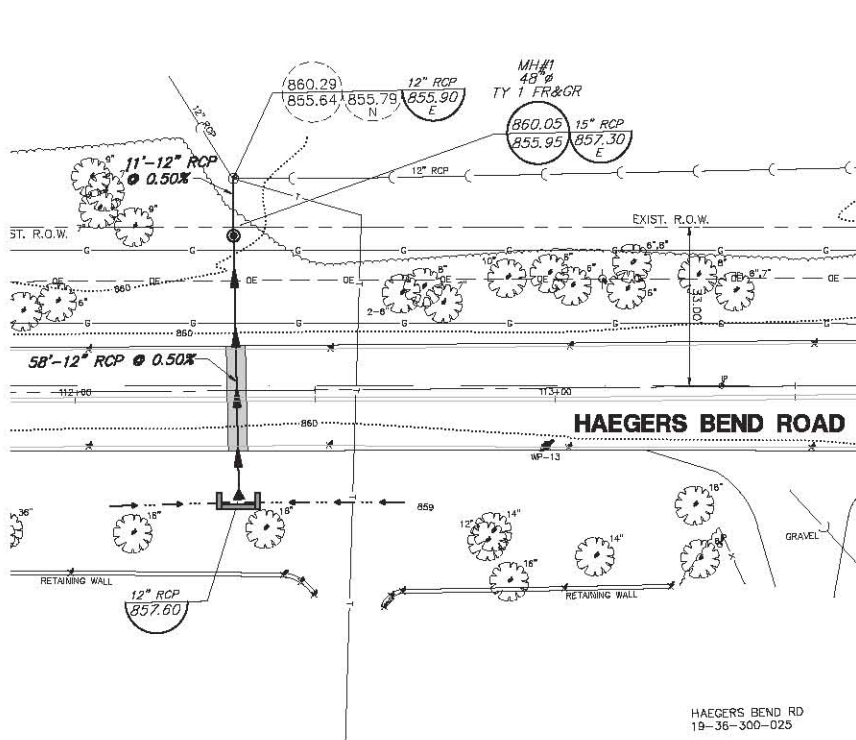
EXISTING/DEMO LEGEND

- STORM SEWER REMOVAL
- SAWCUT PAVEMENT (FULL DEPTH)
- DRIVEWAY PAVEMENT REMOVAL (FULL DEPTH)
- TREE OR BRUSH REMOVAL

DEMOLITION NOTES:

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4. PROVIDE BARRICADES ALONG EDGES OF EXCAVATION AT CLOSE OF EACH CONSTRUCTION DAY.
5. WHERE SHOWN EXISTING UTILITY INFORMATION IS APPROXIMATE BASED ON ATLAS INFORMATION PROVIDED BY THE RESPECTIVE UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING JULIE AND FIELD LOCATING/POTHOLING AS NEEDED TO CONFIRM UTILITY DEPTHS PRIOR TO INSTALLATION.

EXISTING CONDITIONS/DEMOLITION



Point Table				
Point #	Northing	Easting	Elevation	Description
13	2000053.05	1004788.91	859.81	WP-MAG-13

PROPOSED LEGEND

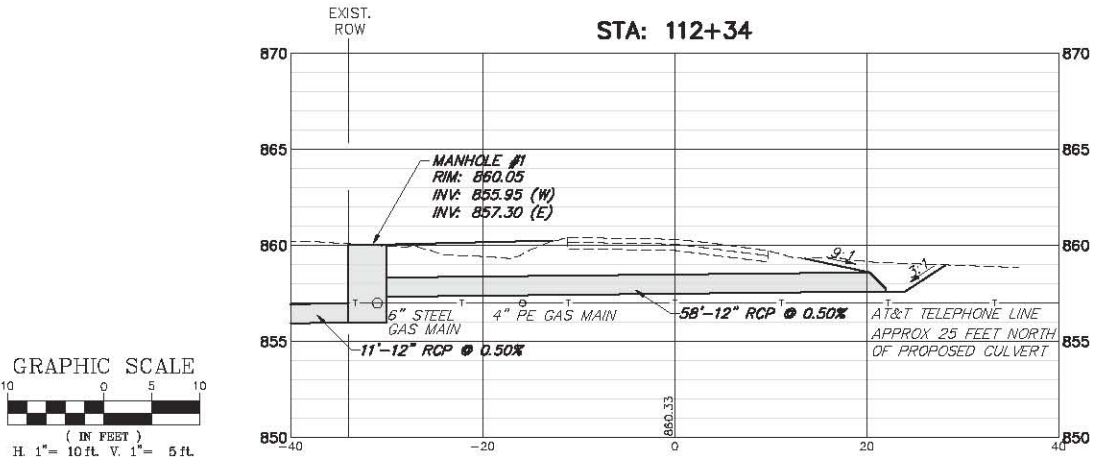
- CLASS D PATCHES, 6" (SPECIAL)
- GRADING & SHAPING DITCHES
- COIR ROLLS
- INLET PROTECTION

NOTES:

1. RESTORE ALL DISTURBED GRASS AREAS WITH 4" TOPSOIL, CLASS 2A SEEDING, & EROSION CONTROL BLANKET.
2. ALL PROPOSED DRAINAGE SWALES SHALL BE BUILT TO MINIMUM 1.0% SLOPE. MINIMUM CROSS SLOPE OF DRAINAGE SWALES SHALL BE 3H:1V.
3. CONTRACTOR SHALL COORDINATE ANY TEMPORARY LOSS OF DRIVEWAY ACCESS WITH RESIDENTS. UNDER NO CIRCUMSTANCES SHALL ANY PROPERTY BE WITHOUT DRIVEWAY ACCESS BEFORE 9:00 AM OR AFTER 4:00 PM.
4. LENGTHS OF CULVERTS SHOWN ARE INCLUSIVE OF LENGTHS OF FLARED END SECTIONS, BUT QUANTITIES WILL BE MEASURED AND PAID FOR SEPARATELY FOR EACH ITEM.
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6. MATERIAL RESULTING FROM TRENCH EXCAVATION MAY NOT BE SIDE CAST INTO WETLANDS OR WATERWAYS.
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PROPOSED CONDITIONS

HAEGERS BEND ROAD CROSS SECTION



CONSTRUCTION SEQUENCING

1. BRUSH/TREE REMOVAL WHERE NECESSARY (CLEAR & GRUB)
2. INSTALL PROPOSED ROADWAY CULVERT, REINFORCED CONCRETE END SECTION, AND MANHOLE
3. COMPLETE GRADING AND SHAPING OF DITCHES & RESTORATION IN THE VICINITY OF THE CULVERT.
4. PERMANENTLY STABILIZE ALL DISTURBED AREAS.

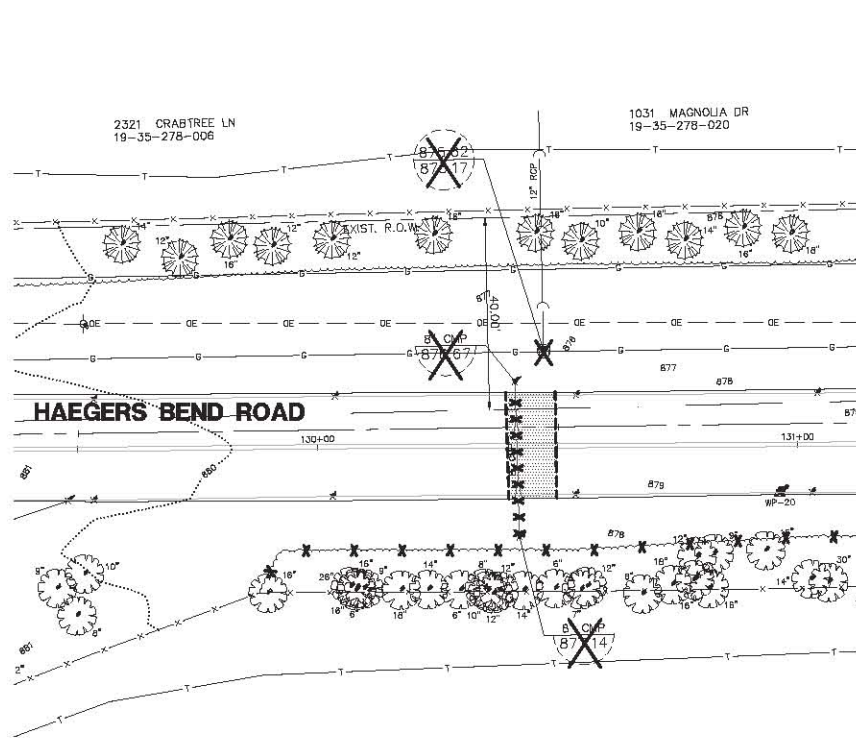
SUMMARY OF DRY UTILITIES

	EXISTING EQUIPMENT	EQUIPMENT TO BE RELOCATED
AT&T CORPORATION	AERIAL CABLES WEST OF HAEGERS BEND ROAD, BURIED CABLES UNDER ROAD WEST TO EAST APPROX 25 FEET NORTH OF CULVERT	NONE
COMCAST		NONE
COMED	OVERHEAD CABLES WEST OF HAEGERS BEND ROAD, 1- PHASE OVERHEAD WIRE WEST OF HAEGERS BEND ROAD	NONE
NICOR GAS	UNDERGROUND GAS MAINS, 4" POLYETHYLENE AND 6" STEEL PIPE, WEST OF HAEGERS BEND ROAD	NONE

PLAN & CROSS SECTION - HAEGERS BEND - STA 112+34

2015 ROAD PROGRAM
VARIOUS CULVERT REPLACEMENTS
VILLAGE OF BARRINGTON HILLS, ILLINOIS

								FILE: 9355.141-PR1.dwg				SHEET NUMBER: 4					
								DRAWN BY: GW3 DATE: 03.09.15									
								GHA PROJECT # 9355.141									
								CHECKED BY: DJS DATE: 03.09.15				SCALE: 1' = 20'					
NO.		BY		DATE		REVISION		NO.		BY		DATE		REVISION		OF 9 SHEETS	



Point Table				
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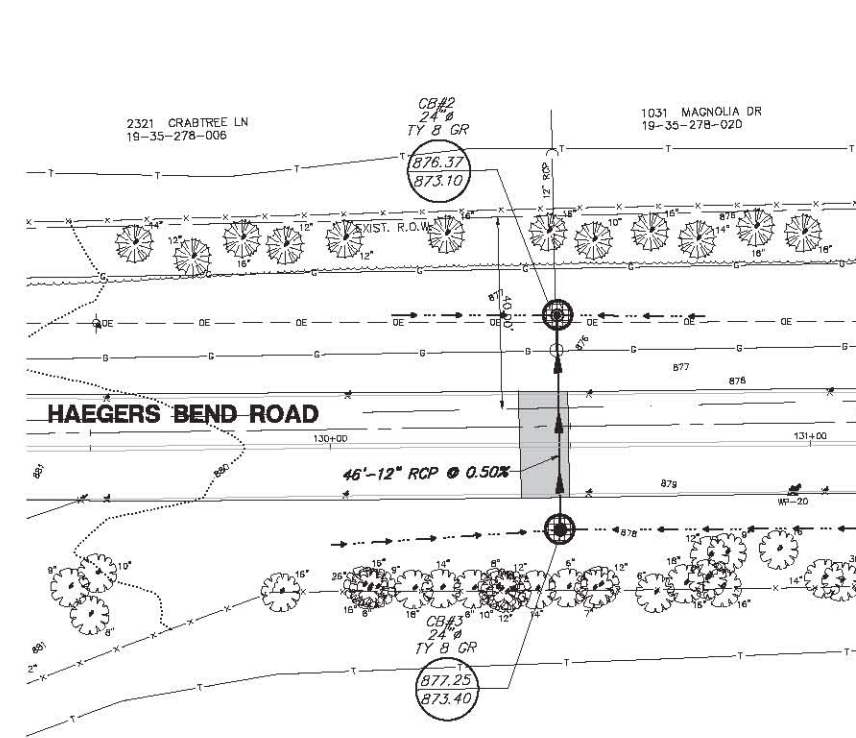
EXISTING/DEMO LEGEND

- STORM SEWER REMOVAL
- SAWCUT PAVEMENT (FULL DEPTH)
- DRIVEWAY PAVEMENT REMOVAL (FULL DEPTH)
- TREE OR BRUSH REMOVAL

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EXISTING CONDITIONS/DEMOLITION



Point Table				
Point #	Northing	Easting	Elevation	Description
20	2001851.02	1004822.14	879.01	WP-MAG-20

PROPOSED LEGEND

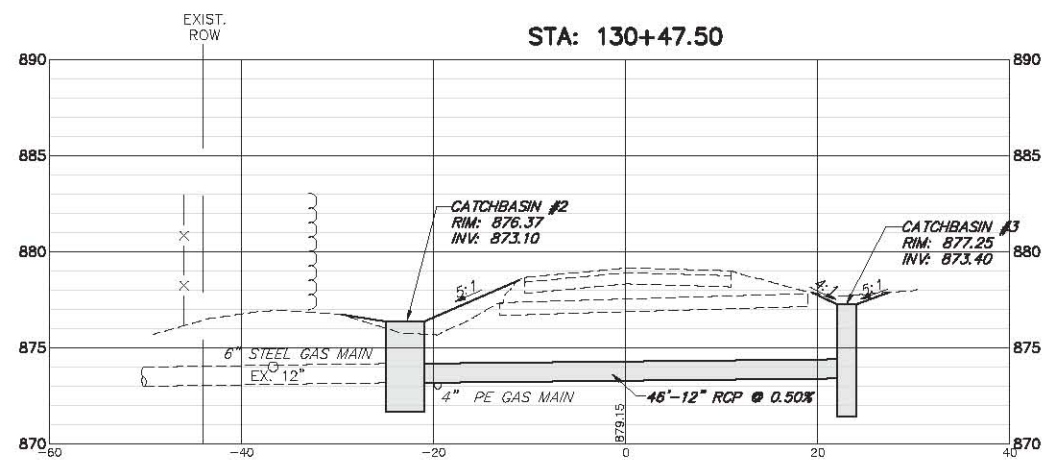
- CLASS D PATCHES, 6" (SPECIAL)
- GRADING & SHAPING DITCHES
- COIR ROLLS
- INLET PROTECTION

NOTES:

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PROPOSED CONDITIONS

HAEGERS BEND ROAD CROSS SECTION

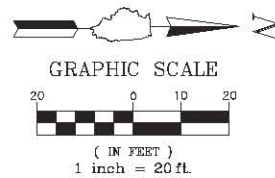
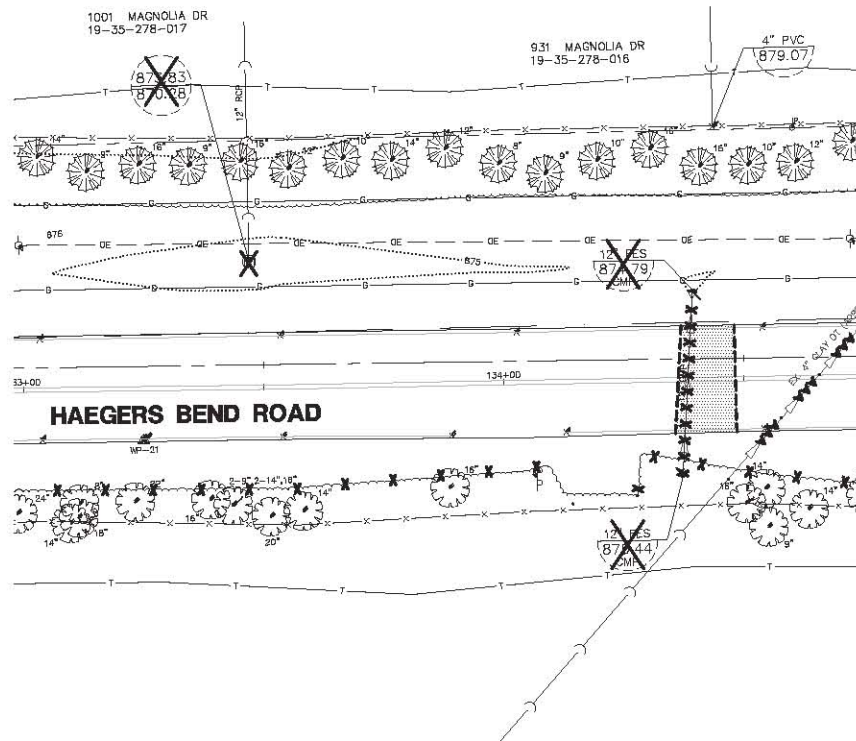


CONSTRUCTION SEQUENCING

- BRUSH/TREE REMOVAL WHERE NECESSARY (CLEAR & GRUB)
- REMOVE EXISTING CULVERT.
- INSTALL PROPOSED ROADWAY CULVERT & CATCH BASINS.
- COMPLETE GRADING AND SHAPING OF DITCHES & RESTORATION IN THE VICINITY OF THE CULVERT.
- PERMANENTLY STABILIZE ALL DISTURBED AREAS.

SUMMARY OF DRY UTILITIES

	EXISTING EQUIPMENT	EQUIPMENT TO BE RELOCATED
AT&T CORPORATION	BURIED CABLES ON THE EAST AND WEST SIDE OF HAEGERS BEND ROAD APPROX 45 AND 62 FEET RESPECTIVELY FROM THE CENTERLINE	NONE
COMCAST		NONE
COMED	OVERHEAD CABLES WEST OF HAEGERS BEND ROAD, 1- PHASE OVERHEAD WIRE WEST OF HAEGERS BEND ROAD	NONE
NICOR GAS	UNDERGROUND GAS MAINS, 4" POLYETHYLENE AND 6" STEEL PIPE, WEST OF HAEGERS BEND ROAD	NONE



Point Table				
Point #	Northing	Easting	Elevation	Description
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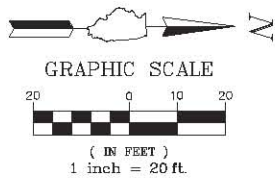
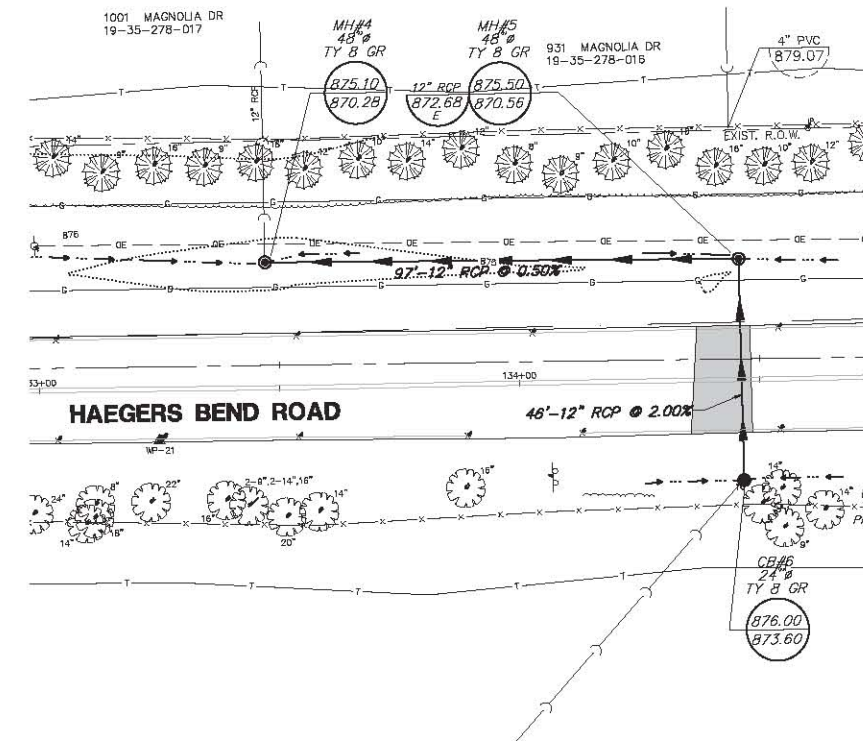
EXISTING/DEMO LEGEND

- STORM SEWER REMOVAL
- SAWCUT PAVEMENT (FULL DEPTH)
- DRIVEWAY PAVEMENT REMOVAL (FULL DEPTH)
- TREE OR BRUSH REMOVAL

DEMOLITION NOTES:

1. AS AN INITIAL ORDER OF OPERATION TRAFFIC CONTROL SIGNS MUST BE IN PLACE PRIOR TO ANY CONSTRUCTION OPERATIONS. A MINIMUM OF ONE LANE OF TRAFFIC MUST REMAIN OPEN AT ALL TIMES. PROVIDE FLAGGERS WHENEVER CONSTRUCTION OPERATIONS LIMIT TRAFFIC FLOW TO ONE LANE.
2. INLET FILTER BASKETS SHALL BE PLACED IN ALL DRAINAGE STRUCTURES PRIOR TO INITIATION OF CONSTRUCTION ACTIVITIES AND REMAIN IN PLACE THROUGHOUT CONSTRUCTION.
3. SAWCUT PAVEMENT (FULL DEPTH) WHERE NOTED AS MARKED IN FIELD BY ENGINEER.
4. PROVIDE BARRICADES ALONG EDGES OF EXCAVATION AT CLOSE OF EACH CONSTRUCTION DAY.
5. WHERE SHOWN EXISTING UTILITY INFORMATION IS APPROXIMATE BASED ON ATLAS INFORMATION PROVIDED BY THE RESPECTIVE UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING JULIE AND FIELD LOCATING/POTHOLING AS NEEDED TO CONFIRM UTILITY DEPTHS PRIOR TO INSTALLATION.

EXISTING CONDITIONS/DEMOLITION



Point Table				
Point #	Northing	Easting	Elevation	Description
21	2002079.74	1004824.86	878.24	WP-MAG-21

PROPOSED LEGEND

- CLASS D PATCHES, 6" (SPECIAL)
- GRADING & SHAPING DITCHES
- COIR ROLLS
- INLET PROTECTION

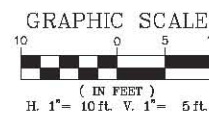
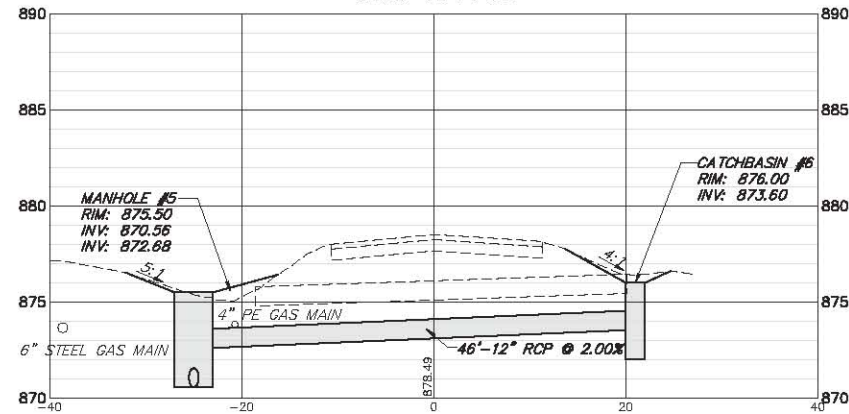
NOTES:

1. RESTORE ALL DISTURBED GRASS AREAS WITH 4" TOPSOIL, CLASS 2A SEEDING, & EROSION CONTROL BLANKET.
2. ALL PROPOSED DRAINAGE SWALES SHALL BE BUILT TO MINIMUM 1.0% SLOPE. MINIMUM CROSS SLOPE OF DRAINAGE SWALES SHALL BE 3H:1V.
3. CONTRACTOR SHALL COORDINATE ANY TEMPORARY LOSS OF DRIVEWAY ACCESS WITH RESIDENTS. UNDER NO CIRCUMSTANCES SHALL ANY PROPERTY BE WITHOUT DRIVEWAY ACCESS BEFORE 9:00 AM OR AFTER 4:00 PM.
4. LENGTHS OF CULVERTS SHOWN ARE INCLUSIVE OF LENGTHS OF FLARED END SECTIONS, BUT QUANTITIES WILL BE MEASURED AND PAID FOR SEPARATELY FOR EACH ITEM.
5. WHERE SHOWN, RIGHT-OF-WAY LIMITS AND PROPERTY BOUNDARIES ARE UNPROVEN BUT BASED ON FOUND PROPERTY CORNERS AND OTHER MONUMENTATION. A BOUNDARY SURVEY WAS NOT COMPLETED WITH THIS TOPOGRAPHIC SURVEY.
6. MATERIAL RESULTING FROM TRENCH EXCAVATION MAY NOT BE SIDE CAST INTO WETLANDS OR WATERWAYS.
7. IN WETLAND AREAS, THE TRENCH SHALL BE BACKFILLED WITH TOPSOIL EXCAVATED FROM THE TRENCH IN THE SAME STRATIFICATION IN WHICH IT WAS REMOVED.
8. IN WETLAND AREAS, GRAVEL BEDDING CANNOT BE USED AS BACKFILL MATERIAL IN THE TRENCH.

PROPOSED CONDITIONS

HAEGERS BEND ROAD CROSS SECTION

STA: 134+46



CONSTRUCTION SEQUENCING

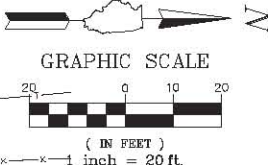
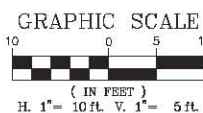
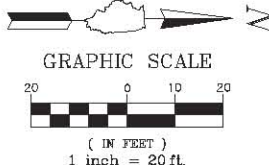
1. BRUSH/TREE REMOVAL WHERE NECESSARY (CLEAR & GRUB)
2. REMOVE EXISTING CULVERT.
3. INSTALL PROPOSED STORM SEWERS AND MANHOLES.
4. CONNECT EXISTING DRAIN TILE TO CB #6
5. COMPLETE GRADING AND SHAPING OF DITCHES & RESTORATION IN THE VICINITY OF THE CULVERT.
6. PERMANENTLY STABILIZE ALL DISTURBED AREAS.

SUMMARY OF DRY UTILITIES

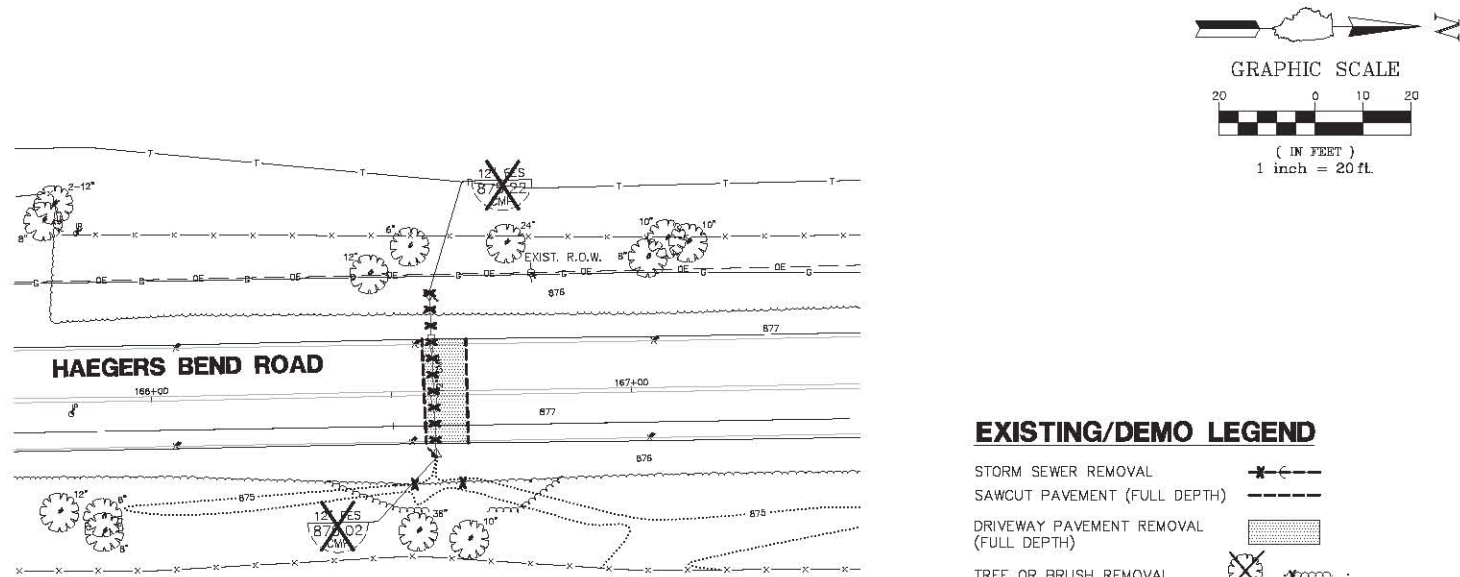
	EXISTING EQUIPMENT	EQUIPMENT TO BE RELOCATED
AT&T CORPORATION	BURIED CABLES ON THE WEST SIDE OF HAEGERS BEND ROAD APPROX 64 FEET FROM THE CENTERLINE	NONE
COMCAST		NONE
COMED	OVERHEAD CABLES WEST OF HAEGERS BEND ROAD, 1- PHASE OVERHEAD WIRE WEST OF HAEGERS BEND ROAD	NONE
NICOR GAS	UNDERGROUND GAS MAINS, 4\"/>	NONE

NO.	BY	DATE	REVISION

FILE: 9355.141-PR1.dwg	GHA PROJECT # 9355.141
DRAWN BY: GW3 DATE: 03.09.15	CHECKED BY: DJS DATE: 03.09.15
SCALE: 1"=20'	



	EXISTING EQUIPMENT	EQUIPMENT TO BE RELOCATED
AT&T CORPORATION	BURIED CABLES ON THE WEST SIDE OF HAEGER BEND ROAD APPROX 48 FEET FROM THE CENTERLINE	NONE
COMCAST		NONE
COMED	OVERHEAD CABLES WEST OF HAEGER'S BEND ROAD, 1- PHASE OVERHEAD WIRE WEST OF HAEGER'S BEND ROAD	NONE
NICOR GAS	UNDERGROUND GAS MAIN, 6" STEEL PIPE, WEST OF HAEGER'S BEND ROAD	NONE



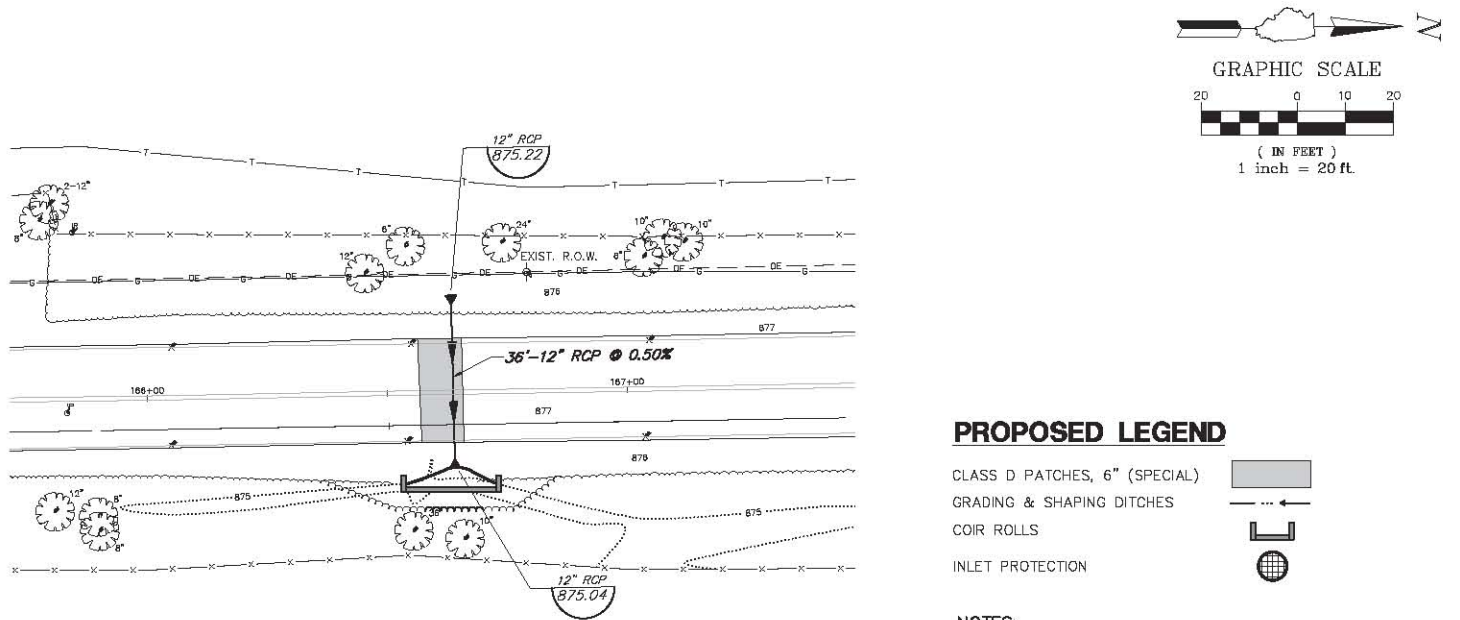
EXISTING/DEMO LEGEND

- STORM SEWER REMOVAL
SAWCUT PAVEMENT (FULL DEPTH)
DRIVEWAY PAVEMENT REMOVAL (FULL DEPTH)
TREE OR BRUSH REMOVAL

DEMOLITION NOTES:

1. AS AN INITIAL ORDER OF OPERATION TRAFFIC CONTROL SIGNS MUST BE IN PLACE PRIOR TO ANY CONSTRUCTION OPERATIONS. A MINIMUM OF ONE LANE OF TRAFFIC MUST REMAIN OPEN AT ALL TIMES. PROVIDE FLAGGERS WHENEVER CONSTRUCTION OPERATIONS LIMIT TRAFFIC FLOW TO ONE LANE.
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EXISTING CONDITIONS/DEMOLITION



PROPOSED LEGEND

- CLASS D PATCHES, 6" (SPECIAL)
GRADING & SHAPING DITCHES
COIR ROLLS
INLET PROTECTION

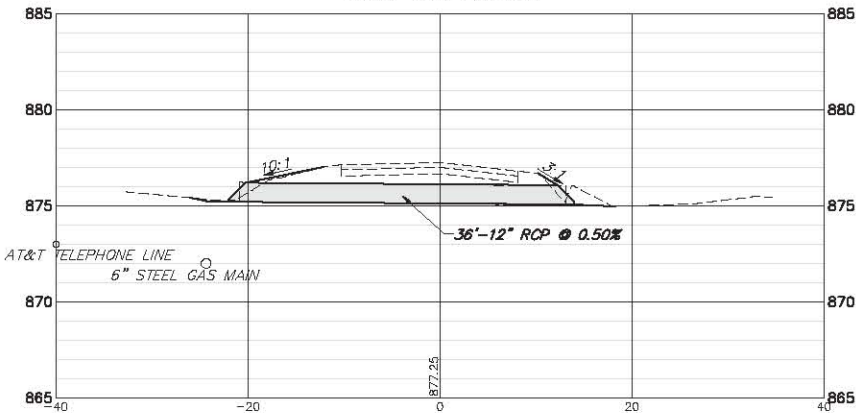
NOTES:

1. RESTORE ALL DISTURBED GRASS AREAS WITH 4" TOPSOIL, CLASS 2A SEEDING, & EROSION CONTROL BLANKET.
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8. IN WETLAND AREAS, GRAVEL BEDDING CANNOT BE USED AS BACKFILL MATERIAL IN THE TRENCH.

PROPOSED CONDITIONS

HAEGERS BEND ROAD CROSS SECTION

STA: 166+63.70

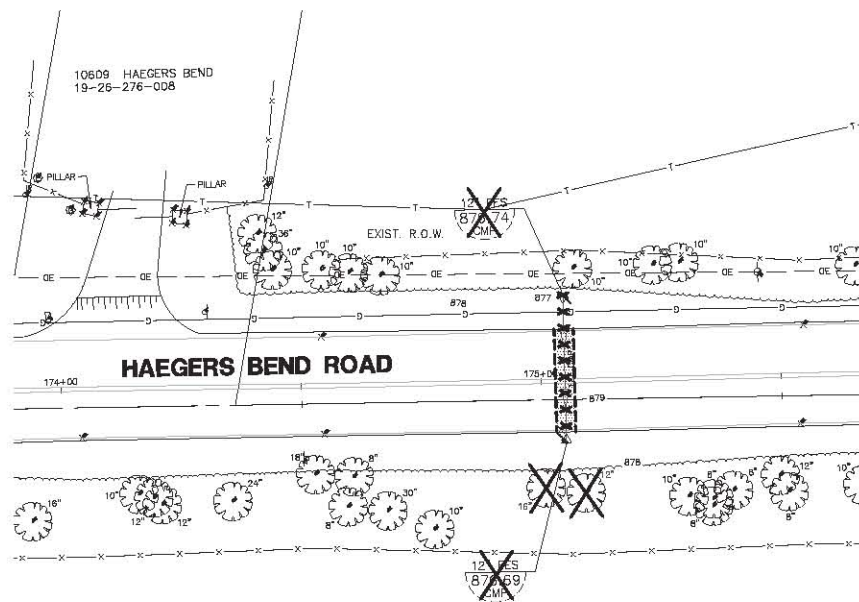


CONSTRUCTION SEQUENCING

1. BRUSH/TREE REMOVAL WHERE NECESSARY (CLEAR & GRUB)
2. REMOVE EXISTING CULVERT.
3. INSTALL PROPOSED ROADWAY CULVERT & REINFORCED CONCRETE END SECTION.
4. COMPLETE GRADING AND SHAPING OF DITCHES & RESTORATION IN THE VICINITY OF THE CULVERT.
5. PERMANENTLY STABILIZE ALL DISTURBED AREAS.

SUMMARY OF DRY UTILITIES

	EXISTING EQUIPMENT	EQUIPMENT TO BE RELOCATED
AT&T CORPORATION	BURIED CABLES ON THE WEST SIDE OF HAEGERS BEND ROAD APPROX 44 FEET FROM THE CENTERLINE	NONE
COMCAST		NONE
COMED	OVERHEAD CABLES WEST OF HAEGERS BEND ROAD, 1- PHASE OVERHEAD WIRE WEST OF HAEGERS BEND ROAD	NONE
NICOR GAS	UNDERGROUND GAS MAIN, 6" STEEL PIPE, WEST OF HAEGERS BEND ROAD	NONE



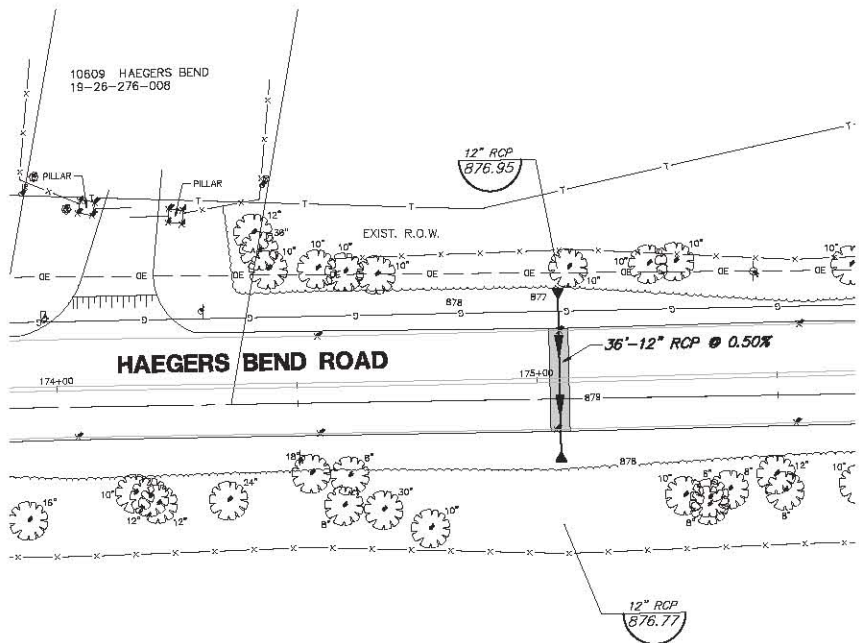
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DRIVEWAY PAVEMENT REMOVAL (FULL DEPTH)
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EXISTING CONDITIONS/DEMOLITION



PROPOSED LEGEND

- CLASS D PATCHES, 6" (SPECIAL)
GRADING & SHAPING DITCHES
COIR ROLLS
INLET PROTECTION

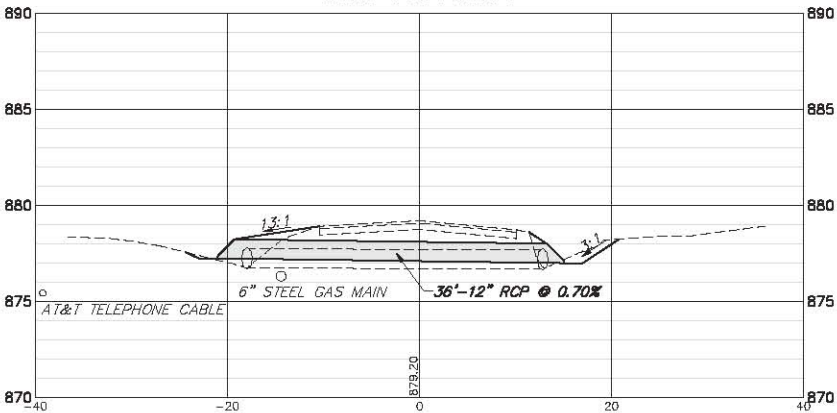
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2. ALL PROPOSED DRAINAGE SWALES SHALL BE BUILT TO MINIMUM 1.0% SLOPE. MINIMUM CROSS SLOPE OF DRAINAGE SWALES SHALL BE 3H:1V.
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PROPOSED CONDITIONS

HAEGERS BEND ROAD CROSS SECTION

STA: 175+04.70



CONSTRUCTION SEQUENCING

1. BRUSH/TREE REMOVAL WHERE NECESSARY (CLEAR & GRUB)
2. REMOVE EXISTING CULVERT.
3. INSTALL PROPOSED ROADWAY CULVERT & REINFORCED CONCRETE END SECTION.
4. COMPLETE GRADING AND SHAPING OF DITCHES & RESTORATION IN THE VICINITY OF THE CULVERT.
5. PERMANENTLY STABILIZE ALL DISTURBED AREAS.

SUMMARY OF DRY UTILITIES

	EXISTING EQUIPMENT	EQUIPMENT TO BE RELOCATED
AT&T CORPORATION	BURIED CABLES ON THE WEST SIDE OF HAEGERS BEND ROAD APPROX 39 FEET FROM THE CENTERLINE	NONE
COMCAST		NONE
COMED	OVERHEAD CABLES WEST OF HAEGERS BEND ROAD, 1- PHASE OVERHEAD WIRE WEST OF HAEGERS BEND ROAD	NONE
NICOR GAS	UNDERGROUND GAS MAIN, 6" STEEL PIPE, WEST OF HAEGERS BEND ROAD	NONE

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: May 18, 2015

Re: Cuba Road Bridge Replacement Project
Status Update

The following is a summary of the current status of the Cuba Road Bridge project.

IDOT Coordination & Schedule

On May 5, 2015 we were notified by IDOT that the letting date for the project had been moved back from June 12, 2015 to July 31, 2015 as the ROW had not yet been certified. Progress has been made in negotiations with the homeowners as described in the next section; however, agreements with the homeowners regarding the ROW cannot be finalized until the Local Agency agreements have been approved by IDOT. These agreements were approved by the Village Board in March, signed agreements were provided to IDOT District One on April 3. IDOT District One approved the agreements and forwarded them to IDOT headquarters in Springfield on April 10, where they still await approval.

This update to the letting schedule may make it difficult to reopen the bridge prior to winter assuming typical weather and construction progress. Additional construction costs may result due to work taking place during winter conditions. Below is an outline of the anticipated schedule:

- Letting Date/Bid Opening July 31, 2015
- Approximate Construction Start Date September 15, 2015
- Projected Bridge Opening Date January 31, 2015*

*Assumes typical working day schedule in September-November, with 10 working days per month in December and January.

Land Acquisition Process

The right-of-way acquisition/negotiation process is in progress and being led by Mathewson Right-of-Way Company. We are coordinating with the Village Attorney to determine the appropriate way to provide this information to the Village Board for approval without jeopardizing the negotiations in any way. Two properties are of note:

- One of the properties involved is currently in foreclosure, which can add several months to the process. As this property is located on an adjacent intersection and does not include the bridge itself, we are pursuing revisions to the plan that will eliminate the need for improvements beyond the existing pavement so that no ROW would be necessary.
- GHA has provided a plat of dedication to the Cuba Township Highway Department to dedicate the required right-of-way on their parcel adjacent to the project. This portion of the right-of-way is on a section of Cuba Road that the Township maintains. I have been in touch with the Highway Commissioner and the Township Attorney and neither anticipate any issues with this request.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
RIGHT OF WAY PLANS
FOR PROPOSED
FEDERAL AID HIGHWAY

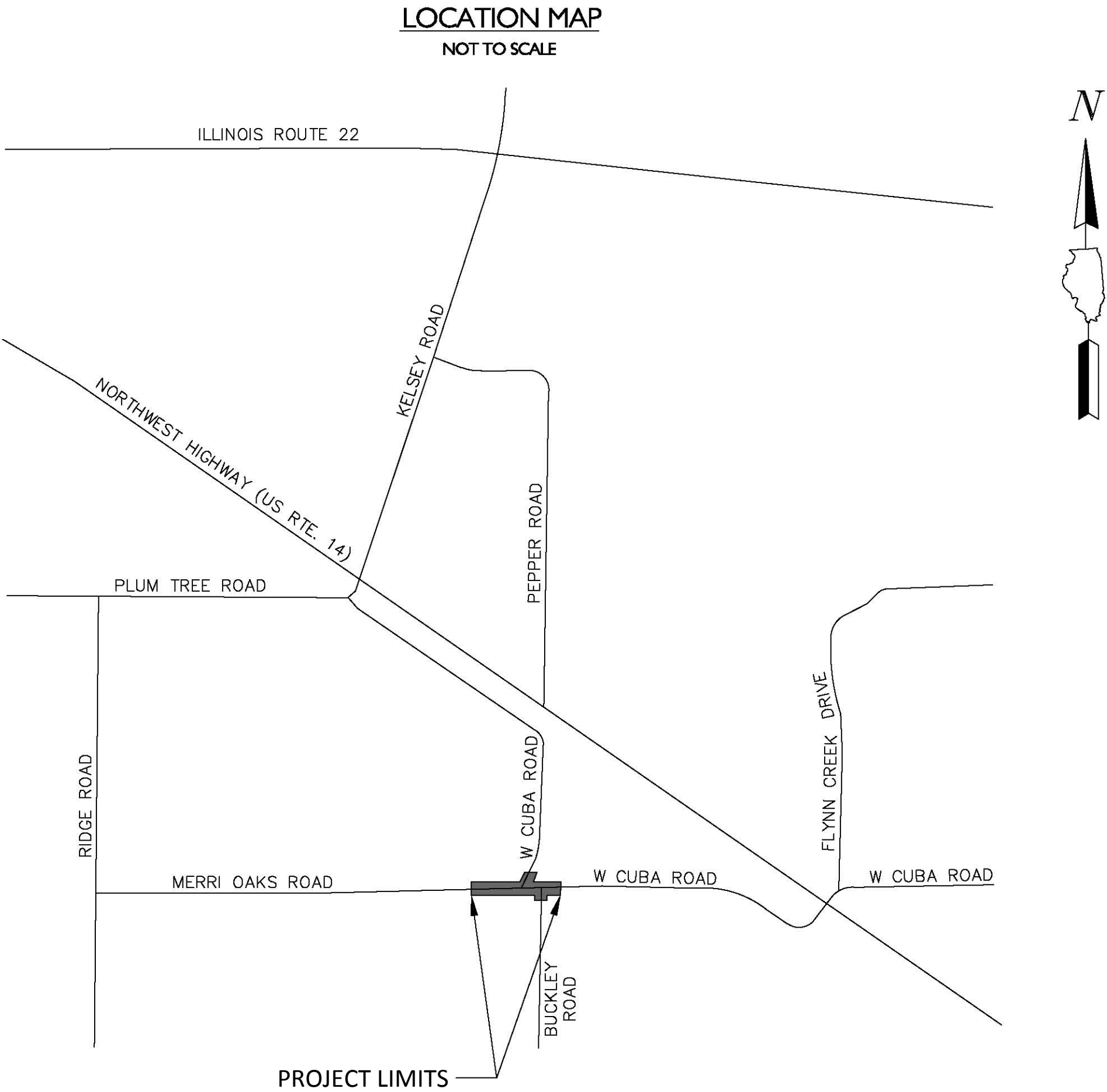
F.A.U. ROUTE 1260
SECTION 12-00020-00-BR
LAKE COUNTY
JOB NO. R-91-006-13
CONTRACT 61B04

PARCEL NUMBER	OWNER'S NAME	SHEET NUMBER	PROPERTY ACQUIRED BY
0001	ROGER J. CHERRY AND TERESA H. CHERRY TRUSTEES OF THE ROGER J. CHERRY 1998 LIVING TRUST	2	
0002	JOHN C. ADAMS AND JULIE A. ADAMS, TRUSTEES OF THE JOHN C. ADAMS LIVING TRUST	2	
0003	LEIGH A. SELLARI AND VICTOR CERDA	2	
0004	THOMAS DIDOMENICO AND SHARON DIDOMENICO	2	
0005	THE TOWNSHIP OF CUBA	2	

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES.

CONTRACT NO. 61B04



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED _____ 20 ____

DISTRICT ENGINEER

EXAMINED _____ 20 ____

DISTRICT LAND ACQUISITION ENGINEER

PASSED _____ 20 ____

DISTRICT CHIEF OF PLOTS AND PLANS

REVIEWED _____ 20 ____

CENTRAL BUREAU RIGHT OF WAY PLANS ENGINEER

APPROVED _____ 20 ____

ENGINEER OF LAND ACQUISITION



MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

625 Forest Edge Drive, Vernon Hills, IL 60061

TEL 847.478.9700 ■ FAX 847.478.9701

www.gha-engineers.com

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: May 18, 2015

Re: 2015 Drainage Program
Status Update

Merri Oaks Road Construction Update

Construction of the Merri Oaks Road project resumed the week of May 9th, 2015 after construction operations were put on hold due to the seasonal weight restrictions. The contractor is currently working on the portions of storm sewer that are directionally drilled on the south side of Merri Oaks Road in order to protect Heritage Trees within the ROW.

To date we have not received a response regarding the "Alternate Bid Items" listed on the plans for which an easement has been requested to work on private property on the north side of Merri Oaks Road. If a response is not received this portion of the work will be omitted.

Steeplechase Road Drainage

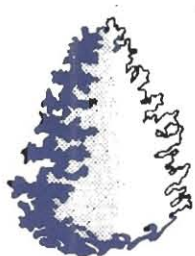
At the February meeting of the Roads & Bridges Committee standing water within the Steeplechase Road parkway near the King Pond was discussed. Options were given to address this condition, including regrading the ditchline or lowering the adjacent roadway culvert to provide additional slope. It was agreed to revisit this condition later in the year when more information would be available regarding the costs for the Merri Oaks Road project and remaining Drainage Management funds available.

Lakeview Lane Drainage

At the March meeting of the Roads & Bridges Committee there was discussion regarding persistent standing water in the ditch lines of Lakeview Lane. Based on existing topography the most viable option would be a drain tile or storm sewer to Heather Lake, which would either need to be a private improvement or include an easement on private property. Recently a quote was forwarded to the Village by a resident of Lakeview Lane from Doering Landscape Company for approximately \$24,840 to install approximately 575' of 8" PVC drain tile. Further clarification and discussion of a few items are needed to determine what further action the Village may take:

- The Village is subject to competitive bidding and prevailing wage requirements. While the quote provided could be accepted and pursued by a private homeowner the Village would need to bid the project competitively.
- Based on preliminary calculations of the tributary area, the peak 10-year flow rate to this area would require a 21" storm sewer (the 10-year event is the design standard for storm sewer). The peak 2-year flow rate would require a 15" diameter storm sewer. As the 8" pipe size proposed is significantly below the 2-year flow rate requirement, ponding water would still result for extended periods of time after storm events, though the water would gradually be drawn down by the 8" drain tile.

- Even the smaller scope of work proposed would represent a significant percentage of the typical Drainage Management portion of the Roads & Bridges budget. If the Village undertakes the project further consideration is needed to determine when funds can be budgeted.



DOERING LANDSCAPE COMPANY

April 8, 2015

Mr. John Keady
47 Lake View Lane
Barrington Hills, IL 60010

Phone: 563-505-7674

Dear Mr. Keady:

In conjunction with the estimate as shown on the attached pages, we propose to furnish the following landscape materials and labor:

Area One - Drain Tile Installation from Front Ditch Area Holding Water to Lake in Rear Yard.

Drain Tile:

Delivery and installation of approximately 575 linear feet of SDR 35 solid 8" drain tile, 1 48" x 24" concrete basin and 1 24" beehive grate (steel). Price includes all connectors and fittings.

Remove:

- Excess soil (all branches are not included in debris removal)
- Haul debris from site

Install:

- Seed
- Fertilizer
- 10 Straw bales

Price - Area One

\$24,840.00 □

Mr. Keady
April 8, 2015

Page 2

Area Two - Option.

Drain Tile:

Delivery and installation of approximately 150 linear feet of solid 4" PVC drain tile with gravel required to pick up water in wet area and by road and connect to basin. Price includes all connectors and fittings.

Price - Area Two

\$2,700.00 □

Black Dirt:

Some pulverized black dirt may be needed to bring the planting bed and lawn areas up to grade. Pulverized black dirt will be delivered and installed at a rate of \$55.00 per yard and will be billed based on the number of yards used.

Debris Removal

If additional debris needs to be removed from site, or if homeowner requests additional product to be removed by Doering vehicles, a charge of \$50.00 per cubic yard will be invoiced. Final price will be based on actual number of cubic yards hauled from site.

Guarantee - Disclaimer:

All hardscape and workmanship are fully guaranteed for one (1) full year from date of completion. This guarantee does not include any damage caused by other contractors on site, snow plow damage to landscaped areas, or any acts of nature. We disclaim liability for **any** damage to irrigation and lighting systems or any other built-in systems.

Permits/Drawings, etc.

Any additional costs incurred by Doering Landscape for services required to execute the landscape design such as local permits, civil engineering drawings, village presentation meetings or additional architectural drawing are not included in the above-listed quote and will be performed and billed on a time and material basis.

J.U.L.I.E.:

Doering Landscape will call JULIE and have your property located for underground utilities prior to the start of any construction. **Any underground utilities that cannot be located by JULIE (invisible dog fencing, inground sprinkler systems and lighting) will be the sole responsibility of the homeowner.**

Mr. Keady
April 8, 2015

Page 3

Method of Payment:


Forty percent down upon signing of the contract with the **balance** due on the date of completion. Please note we DO NOT accept credit cards. If the balance is not paid within ten (10) days following the date of the invoice, interest shall be charged at the rate of one and one-half percent (1 ½%) per month from the date payment became due until the account is fully paid. All material furnished hereunder shall remain our property until we have received full payment.

Upon acceptance of this proposal, please sign the original proposal and return it **in its entirety** to our office. Please retain the copy for your records.

Thank you for the opportunity to submit this proposal to you. If you have any questions, please feel free to contact me at 381-5115.

Sincerely,

DOERING LANDSCAPE COMPANY



Ron O'Hara
General Manager

ACCEPTANCE:

By: _____

Date: _____

Our property has: Sprinkler System _____
 Invisible Dog Fence _____
 Landscape Lighting _____

VILLAGE OF BARRINGTON HILLS
112 ALGONQUIN ROAD
BARRINGTON HILLS, ILLINOIS 60010-5199
(847)551-3000

Revised: May 05, 2015

[HOME](#)[ABOUT](#)[LEGISLATION](#)[COMMITTEES](#)[DISTRICT](#)[MEDIA](#)[CONTACT](#)[SURVEY 2015](#)

IDOT Allocates \$63 Million Toward Longmeadow Parkway Bridge Across Fox River

on Friday, May 15, 2015



State Representative Mike Tryon (R-Crystal Lake) has announced that close to \$63 million in Illinois Department of Transportation (IDOT) funds have been earmarked over the next few years to help provide motorists with a new bridge across the Fox River via Longmeadow Parkway in Kane County.

According to Tryon, the IDOT 2016-2019 Highway Improvement Program for District 1 includes seven different projects that will move the Longmeadow Parkway bridge project forward. "For the commuters who must cross the Fox River daily to get to and from work and who do not use I-90, they are limited in this area to the Algonquin Road and Route 72 bridges," said Tryon. "Traffic during peak periods has been horrible for years and I am

thrilled to see this project finally moving forward in earnest. This is a project that will provide a great deal of traffic relief not just for Kane County, but for the entire region."

Improvements tied to the Longmeadow Parkway project include:

Route/Street	Location	Improvements	City
Longmeadow Pkwy/Bolz Road	Huntley Rd at Boyer, East to Randall Rd	New Construction	Algonquin
Longmeadow Pkwy/Bolz Road	East of Randall Road to East of IL Route 31	Additional Lanes, Intersection Improvements	Algonquin
Longmeadow Pkwy/Bolz Road	East of Randall Road to East of Route 31	Construction Engineering	Algonquin
Longmeadow Pkwy/Bolz Road	East of IL 31 to East of IL 25	Intersection Improvement, New Construction (new bridge)	Carpentersville/Barrington Hills
Longmeadow Pkwy/Bolz Road	East of IL 31 to East of IL 25	Construction Engineering	Carpentersville/Barrington Hills
Longmeadow Pkwy/Bolz Road	East of IL 25 to IL 62	Intersection Improvement, New Construction	Carpentersville
Longmeadow Pkwy/Bolz Road	East of IL 25 to IL 62	Construction Engineering	Barrington Hills

"Since I began representing Northern Kane County three years ago, the Longmeadow Parkway improvement project has been their top priority," Tryon said. "Senator McConnaughay and I meet regularly with our mayors and municipal officials, and their commitment to this project has never wavered. We brought their message to IDOT and are thrilled to know that the Department of Transportation now also considers Longmeadow Parkway as a priority."

Newsletter Signup

Sign Me Up

 Search

Office Information:

Springfield Office:

200-7N Stratton Office Building
Springfield, IL 62706
(217) 782-0432
(217) 782-1141 FAX

District Office:

1500 Carlemont
Suite D
Crystal Lake, IL 60014
(815) 459-6453
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Protect Our Schools



News Archive

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AUDIO: Representative Tryon Talks About the Econom...

IDOT Allocates \$63 Million Toward Longmeadow Park...

In addition to the traffic mitigation benefits, Tryon pointed to the improved opportunities for economic development that would result from the project's completion. "Especially for the Villages of Algonquin and Carpentersville and for East and West Dundee, this third accessway across the Fox will open the door to future business growth and prosperity."

To hear more from Rep. Tryon on this issue, click [here](#).

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Legislative Update: May 11, 2015

Tryon Releases 2015 Legislative Survey Results

Legislative Update: May 6, 2015

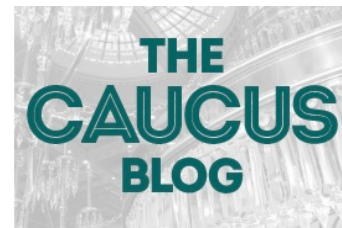
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State Representative Mike Tryon (R-Crystal Lake) will join other local legislators this month for two Senate Bill 16 Impact forums in McHen...



Representative Mike Tryon Asks Residents to File Official Objections to SB16 through www.ilga.gov Web Site

State Representative Mike Tryon (R-Crystal Lake), concerned about a committee hearing that has been scheduled in Springfield to gather tes...



Tryon Co-Sponsors Legislation Opposing Senate Bill that Diverts



Robert Kosin <rkosin@barringtonhills-il.gov>

(no subject)

Robert Kosin <rkosin@barringtonhills-il.gov>

Mon, May 18, 2015 at 8:42 PM

To: "Brian D. Cecola" <bcecola@barringtonhills-il.gov>

Cc: "Dan Strahan P.E." <dstrahan@gha-engineers.com>

Algonquin Longmeadow Still Long Off Despite \$63 Million "Earmark"

First Electric Newspaper LLC
Monday, May 18, 2015

Local legislators' announcements Friday that the Illinois Department of Transportation had "earmarked" \$63 million to build the Longmeadow Parkway over the Fox River South of Algonquin didn't mean construction will get underway soon, according to Kane County Planners. In fact, under current assumptions, work won't start for about a year.

The Longmeadow Parkway would run from Route 62 west to Huntley/Boyer but the way Kane County Division of Transportation engineers have laid it out, construction would really be in four separate pieces. The \$63 million figure comes from adding up all the engineering and construction estimates for all the sections for next five years. IDOT's latest Proposed Highway Improvement Program only plans to spend \$16.2 million on the \$100 million Parkway next year. The money would build the end segments, not the two middle ones which include a toll bridge over the Fox River.

Assistant Director Steve Coffinbarger told FEN Friday KDOT's schedule calls for bids on the Huntley/Boyer to just short of Randall Road leg in late September which, realistically would mean contractors wouldn't be ready to move dirt until November. "That's when it starts to get really cold," said Coffinbarger, too cold for road building. "It might be a mild winter --you never know," he said, "but probably construction won't start until next Spring."

Next year's IDOT planners' timeframe for the other end of the Longmeadow, too, the leg from east of Route 25 to Route 62. That hasn't even been designed yet.

Money for the sections including the bridge and the intersections with both Randall and Route 25 are only "allocated" to a period somewhere between 2017 and 2021. For one thing, KDOT doesn't have an OK for those yet from the Federal Highway Administration. FHA needs to certify the Parkway won't hurt the Buffalo Park, Brunner Family and Fox Shores forest preserves before they can be built. June 7 is the deadline for public comment.

In the pic: Construction on Longmeadow Parkway "Section A" probably won't start until next Spring, according to Kane County officials.

First Electric Newspaper LLC at 10:21 AM

First Electric Newspaper LLC

Pete Gonigam--Publisher. First Electric Newspaper LLC, PO Box 7211 Algonquin, IL. 60102-7211 gonigam@firstelectricnewspaper.com (847) 586-3040

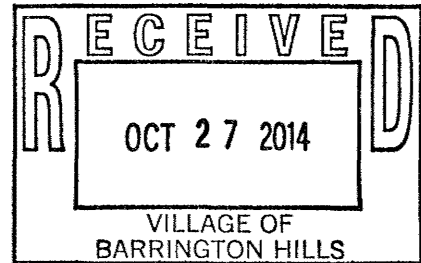


Illinois Department of Transportation

Division of Highways / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

October 21, 2014

The Honorable Martin J. McLaughlin
Village President
Village of Barrington Hills
112 Algonquin Road
Barrington Hills, IL 60010



Dear Village President McLaughlin:

This is a follow up to our correspondence and discussion regarding capacity needs for Illinois Route 62 (Algonquin Road) between Illinois Route 25 and Illinois Route 68. We offer the following information.

Based on the current Average Daily Traffic (ADT) volumes along Illinois Route 62, a capacity improvement (add-lane project) has been identified along Illinois Route 62 from Illinois Route 25 to Illinois Route 68. The estimated cost for construction, engineering, and land acquisition associated with this improvement is approximately \$75,770,000 in 2014 dollars. Funding for construction and preconstruction activities associated with this improvement project are not included in our current FY 2015-2020 Proposed Multi-Modal Transportation Improvement Program.

The Preliminary Engineering and Environmental Studies (Phase I engineering) have not been initiated for this improvement. Due to our limited fiscal resources, our Department does not have funding available in our current FY 2015 Annual Highway Improvement Program to advertise for engineering services for this improvement need. However, in recognition of the regional significance of this improvement as it relates to current and anticipated traffic needs, our Department will make every effort to include this Phase I funding in our upcoming FY 2016-2021 Proposed Multi-Modal Transportation Improvement. We have also included this project in our priorities for funding as part of any future Capital Improvement Programs considered by the Illinois General Assembly.

The Honorable Martin J. McLaughlin
October 21, 2014
Page 2

Once funding for preliminary engineering is available and our agency is able to embark on the preliminary engineering studies for a proposed capacity improvement along Illinois Route 62, the study process will employ Context Sensitive Solutions or "CSS". CSS is an interdisciplinary approach that seeks effective, multi-modal transportation solutions by working with stakeholders to develop, build and maintain cost-effective transportation facilities which fit into and reflect the project's surroundings or its "context." Through early, frequent, and meaningful communication with stakeholders, and a flexible and creative approach to design, the resulting project would improve safety and mobility for the traveling public, while seeking to preserve and enhance the scenic, economic, historic, and natural qualities of the settings through which they pass.

If you have any questions or need additional information, please contact me or Marnie Hooghkirk, Area Programmer, at (847) 705-4075.

Very truly yours,

A handwritten signature in black ink, appearing to read "John Fortmann".

John Fortmann, P.E.
Deputy Director of Highways,
Region One Engineer

**bcc: Board of Trustees
Village Engineer**

President
MARTIN J. McLAUGHLIN

Trustees
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112 ALGONQUIN ROAD
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www.barringtonhills-il.gov

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FACSIMILE
(847) 551-3050

May 5, 2014

Mr. John Fortmann
Region 1 Engineer
Illinois Department of Transportation, District 1
201 West Center Court
Schaumburg, Illinois 60196

Re: IL Rte. 62 (Algonquin Road) Improvements, Village of Barrington Hills


Dear Mr. Fortmann:

I appreciate the meeting with you and your staff on Friday, January 31, 2014 at IDOT District One Offices. It was a candid discussion about the recent Phase I design approval of the Longmeadow Parkway project being pursued by the Kane County Division of Transportation and how that has renewed concerns within the Village of Barrington Hills regarding the capacity of IL Rte. 62 (Algonquin Road). In particular, the existing two-lane section between IL Rte. 68 and IL Rte. 25 continues to see congestion on a daily basis, with an ADT of approximately 22,000. It is anticipated that additional traffic volume from the Longmeadow Parkway project will further add to this congestion.

This anticipated increase in traffic volumes creates a number of concerns for the Village. As congestion increases, motorists increasingly utilize the local roadway network to avoid Algonquin Road traffic, and thereby increase traffic loads on county, village, and private roads which were constructed with the intention of serving only local traffic loads. Increased traffic loadings will further increase the risk associated with crossing IL Rte. 62 at Old Sutton Road, Bateman Road, and Helm Road, which some motorists avoid already due to limited gaps in traffic during peak hours. The Village has seen numerous accidents and fatalities in the past at these intersections. Existing equestrian and pedestrian crossing locations would also be jeopardized by increased traffic volumes. These impacts will result in IL Rte. 62 bisecting the Village, impacting the quality of life for those living or traveling in this area of the Village.

The Village of Barrington Hills for these concerns requests, with no prior expectation of approval or waiver of rights, that the District undertake a study to examine and pursue capacity and safety improvements along IL Rte. 62.

Sincerely,
Village of Barrington Hills


Martin J. McLaughlin
Village President

cc: Board of Trustees

A HOME RULE COMMUNITY

**RESOLUTION SUPPORTING A BRIDGE STUDY CORRIDOR
AND PROJECT IN DUNDEE TOWNSHIP FOR CONSTRUCTION OF THE
LONGMEADOW PARKWAY (BOLZ ROAD) BRIDGE, AND ROUTE 62
TRAFFIC CORRIDOR STUDY FROM ROUTE 68 TO ROUTE 31**

WHEREAS, the Village of Barrington Hills is dedicated to participating in regional traffic planning consistent with the goals of its Comprehensive Plan, and for the betterment of its residents, and the region; and

WHEREAS, significant traffic congestion is presently experienced at the existing Fox River bridge crossings within Dundee and Algonquin Townships and on a number of local and state roadways leading to said crossings; and

WHEREAS, traffic congestion on State Routes within the Village of Barrington Hills negatively impacts property values, safety, and quality of life for the residents of the Village; and

WHEREAS, traffic congestion within the Village of Barrington Hills where Bateman and Sutton Roads cross Route 62 is well documented and recognized by its residents; and

WHEREAS, existing bridge capacities are inadequate for the current and projected development of the area resulting in increased traffic congestion, travel times, air pollution, fuel consumption, risk to pedestrians and bicyclists, and other impacts to existing infrastructure and residences; and

WHEREAS, traffic congestion within the Village of Barrington Hills is directly linked to bridge capacity at the two existing Fox River bridge crossings within Dundee Township; and

WHEREAS, the County of Kane, which includes the municipalities of Barrington Hills and Algonquin within Dundee Township, is projected by the Northeastern Illinois Planning Commission to be one of the fastest growing areas in the six-county northeastern Illinois area; and

WHEREAS, there is underway a feasibility study and environmental assessment under the auspices of Kane County for a future Fox River bridge crossing that could serve both localized and regional traffic needs in Dundee Township north of IL State Route 72 and west of Bolz Road; and

WHEREAS, the Village of Algonquin has fostered a regional transportation planning consensus through what has become known as the Longmeadow Parkway Bridge Study Corridor ("Corridor"); and

WHEREAS, the construction of the Longmeadow Parkway Bridge will most likely forgo the need for other local Fox River bridge projects and thus help to stabilize planning and property values within the Villages of Barrington Hills and Algonquin; and

WHEREAS, the Village of Barrington Hills has supported the planning efforts of the Village of Algonquin and Kane County in developing a consensus for the study of the Corridor; and

WHEREAS, the Village of Barrington Hills has supported study efforts of IDOT, Kane County, and Cook County for the IL State Route 62 traffic corridor from IL State Route 68 to IL State Route 25; and

WHEREAS, the Village of Barrington Hills has a strong equestrian foundational component in its land use and long history of equestrian and other nature activities on private, public, and Forest Preserve property, and that the road systems referenced here effectively bisect the Village.

NOW, THEREFORE BE IT RESOLVED by the President and Board of Trustees of the Village of Barrington Hills, located in Cook, Kane, Lake and McHenry Counties, Illinois as a home rule municipality the following:

Section One. The foregoing recitals are hereby incorporated into this Resolution as findings of the President and Board of Trustees.

Section Two. The President and Board of Trustees in view of the foregone recital support the efforts to obtain funding for the continual feasibility study, environmental assessment, and eventual construction for a future Fox River bridge crossing at or near the western end of Bolz Road from existing Longmeadow Parkway to IL State Route 62 that could serve both the localized and regional traffic needs of the Township of Dundee north of IL State Route 72.

Section Three. The President and Board of Trustees will work towards a regional consensus with the Village of Algonquin, Carpentersville, Kane County, and McHenry County on the design and construction of a Longmeadow Parkway Bridge.

Section Four. The President and Board of Trustees will work towards a local and regional consensus with the residents of Barrington Hills, IDOT, the Village of Algonquin, Cook county, Kane County, and the Cook County Forest Preserve to study the variety of traffic flow solutions to address congestion from IL State Route 68 to IL State Route 25, including the following intersections with IL State Route 62:

Old Sutton Road
Springwood Lane
Bateman Road
Helm Road
Royal Way

Regan Boulevard
Bolz Road as proposed in the Longmeadow Parkway Bridge Study Corridor
Autumn Trail north (West End)
Autumn Trail south (East End)
Private access points along the identified section of IL State Route 62

Section Five. The President and Board of Trustees will work to eliminate or mitigate the impact as may be identified in traffic and regional analysis of an extension of Boltz Road to IL State Route 62 within the Longmeadow Parkway Bridge Study Corridor on existing Barrington Hills' residents and their property.

Section Six. This Resolution recognizes that there is a small but real possibility that the extension of Bolz Road to IL State Route 62 within the Corridor or potential upgrade of IL State 62 could impact certain Barrington Hills residential properties. As such, the Village of Barrington Hills is committed to working with any affected property owner(s) and the State of Illinois to ensure to the extent possible that any affected resident(s) are properly compensated, and that any displaced residents are afforded similar property opportunities within the Village. In the unlikely event a resident must be relocated as part of a detailed regional effort, and suitable alternative property is not readily available in the Village market place, the Village may consider a wide variety of efforts, including the application of annexation, within its planning area to expand the Village proper, creating available property for the affected resident(s).

Section Seven. The Village of Barrington Hills will support and offer its participation with IDOT, the McHenry County Department of Transportation, and the Village of Algonquin to upgrade the intersection of IL State Routes 62 and 31.

Section Eight. The Village of Barrington Hills will ensure through these roadway planning activities, provisions are made for enhanced equestrian and other nature activity access and crossing within the roadway plan area, including Forest Preserve access.

Section Nine. This Resolution shall be in full force and effect from and after its approval and publication according to law for which copies shall be spread across the minutes of the Village and made available to all so interested.

PASSED BY THE PRESIDENT AND BOARD OF TRUSTEES of the Village of Barrington Hills, Illinois at a regular meeting thereof held on the 27 day of March, 2006.

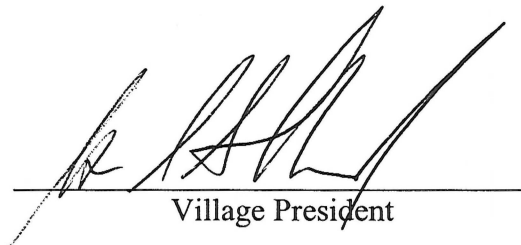
APPROVED THIS 27TH DAY OF MARCH, 2006

AYES: 6, NAYS: 0, ABSENT: 1.

(SEAL)

ATTEST:


Village Clerk


Village President

President
MARTIN J. McLAUGHLIN

Trustees
FRITZ GOHL, Pro-Tem
JOSEPH S. MESSER
KAREN S. SELMAN
PATTY MERONI
COLLEEN KONICEK
MICHAEL HARRINGTON

DOLORES G. TRANDEL, Village Clerk



112 ALGONQUIN ROAD
BARRINGTON HILLS, ILLINOIS 60010-5199
www.barringtonhills-il.gov

TELEPHONE
(847) 551-3000

FACSIMILE
(847) 551-3050

February 25, 2015

Ms. Emily Karry
Director of Planning & Programming
Lake County Division of Transportation
600 W. Winchester Road
Libertyville, IL 60048

Re: US14/Hart Road Improvements
Proposed Sidewalk

Dear Ms. Karry:

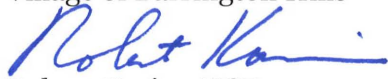
The Village of Barrington Hills understands that a meeting was held at the Lake County Division of Transportation (LCDOT) on Thursday, November 6, 2014 regarding proposed sidewalk improvements that may be added to the scope of the US 14/Hart Road intersection improvements project currently in the design phase. Further, it is understood that a request was made to LCDOT by a Barrington Hills resident with a driveway within the project limits to add pedestrian considerations to the west side of Hart Road. As this section of Hart Road is located within the corporate limits of Barrington Hills (though maintained by LCDOT) we understand that the Village Engineer, Dan Strahan, was asked whether the Village would participate in the costs of a sidewalk or path in this location, in accordance with county funding policies.

Please note that the Village of Barrington Hills does not currently maintain any sidewalks or roadside paths of any kind within its corporate limits. In addition, the Village does not maintain any infrastructure improvements within the right-of-way of any roads maintained by other jurisdictions. While the Village would not object if Lake County DOT pursues further pedestrian connectivity within its right-of-way, for the reasons outlined above the Village of Barrington Hills would not participate in the construction costs or future maintenance of such improvements along Hart Road.

With regard to this specific location, the proposed intersection improvements already constitute a considerable impact to the private properties located in Barrington Hills that adjoin Hart Road. The Village is concerned with the additional impacts that would result from adding a bike path or sidewalk along the west side of Hart Road at this location, not to mention adding considerable delay to a much need project that would relieve congestion at the US 14/Hart Road intersection.

If you have any questions I can be reached at 847-551-3000 or at rkosin@barringtonhills-il.gov.

Sincerely,
Village of Barrington Hills


Robert Kosin, AICP
Director of Administration

A HOME RULE COMMUNITY

cc: Barrington Hills Village Board

President
MARTIN J. McLAUGHLIN

Trustees
FRITZ GOHL, Pro-Tem
JOSEPH S. MESSER
KAREN S. SELMAN
PATTY MERONI
COLLEEN KONICEK
MICHAEL HARRINGTON

DOLORES G. TRANDEL, Village Clerk



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May 12, 2015

Ms. Janell Jensen
Planning Liaison
McHenry County Council of Mayors
16111 Nelson Road
Woodstock, IL 60098

Re: FAU Route Reclassification Requests
Village of Barrington Hills

Dear Ms. Jensen:

Pursuant to Resolution 14-20 approved by the Village of Barrington Hills Board of Trustees in September 2014, last fall the Village submitted requests to the McHenry County Council of Mayors to approve revisions to the functional classification Plum Tree Road and Ridge Road, among other roads. After approval by the Council the requests were forwarded on to IDOT. IDOT provided the attached response letter dated February 27, 2015, which rejected the requested revision to a Local Street but notes that a revision from Minor Arterial to Major Collector would be acceptable.

In accordance with the guidance from IDOT, the Village requests that the following streets be reclassified within the Village limits:

Road	FAU #	Current Classification	Proposed Classification
Plum Tree Road (East of Ridge)	3879	Minor Arterial	Major Collector
Ridge Road	3708	Minor Arterial	Major Collector

If you have any questions I can be reached at 847-551-3000 or at rkosin@vbhil.gov.

Sincerely,
Village of Barrington Hills

Robert Kosin, AICP
Director of Administration

cc: Barrington Hills Village Board

A HOME RULE COMMUNITY



Illinois Department of Transportation

Division of Highways / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

February 27, 2015

Ms. Janell Jensen
McHenry County Council of Mayors
16111 Nelson Road
Woodstock, IL 60098

Re: Functional Classification Revision Request
Haegers Bend Road, Merri Oaks Road, Ridge Road, and Spring Creek Road
Barrington Hills, Illinois

Dear Ms. Jensen:

The Department has reviewed your submittal regarding the Functional Classification Revision request for Haegers Bend Road, Merri Oaks Road, Ridge Road, and Spring Creek Road in the Village of Barrington Hills. We have reviewed these routes individually and have determined the following:

Haegers Bend Road has a 2012 AADT ranging from 2070 to 3280 between County Line Road and River Road. These counts, along with important north-south connectivity between a Major Collector and a Minor Arterial, suggest that Haegers Bend Road does function as a Major Collector, and therefore should not be reclassified as a Local Street.

Merri Oaks Road has a 2012 AADT ranging from 800 to 975 vehicles per day between Ridge Road and US Rte 14. Based on the traffic volumes, the roadway classification could be lowered from a Major Collector to a Local Street. However, the Department would need concurrence from Cuba Township as a portion of the roadway falls within Cuba Township jurisdiction.

Ridge Road has a 2012 AADT ranging from 3570 to 3620 vehicles per day between County Line Road and Plum Tree Road. This road is currently classified as a Minor Arterial; however the AADT suggest that it truly functions as a Major Collector, therefore if the Village wanted to change the classification from a Minor Arterial to a Major Collector that would be acceptable. The Functional Classification of Plum Tree Road from Ridge Road to US Rte 14 would also have to be changed to a Major Collector as well.

Ms. Janell Jensen
February 27, 2015
Page 2

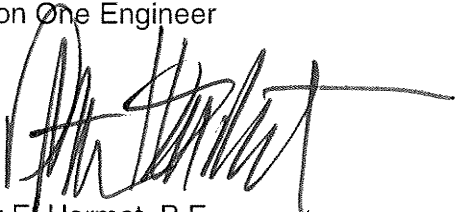
Spring Creek Road has a 2012 AADT ranging from 650 to 1280 vehicles per day between Haegers Bend Road and Ridge Road. This road is currently classified as a Major Collector, however the AADT suggest that it should be downgraded to a local road. This request is being processed by IDOT.

Thank you for your interest in the Functional Classification process.

If you have any questions or need additional information, please contact Pam Heurich, Tracking Manager, at (847) 705-4217.

Very truly yours,

John Fortmann, P.E.
Deputy Director of Highways,
Region One Engineer

By: 
Peter E. Harmet, P.E.
Bureau Chief of Programming

cc: Martin McLaughlin, President, Village of Barrington Hills
Robert Kosin, Village Administrator, Village of Barrington Hills
Travis Lobmaster – IDOT Central Office
Gregory Runyard – IDOT Central Office
Kevin Belgrave – Gewalt Hamilton Associates

Appendix A

Functional Classification Revision Request Template

1. Name(s) of proposed roadway to be reclassified:

Ridge Road

2. Name of agency requesting revision (roadway jurisdiction):

Village of Barrington Hills

3. Contact information (name, title, address, phone and email):

Robert Kosin, Village Administrator, 112 Algonquin Rd, 847-551-3000, rkosin@barringtonhills-il.gov

4. Council(s) of Mayors:

McHenry County

5. County(ies) of proposed roadway to be reclassified:

Lake/McHenry

6. Township(s) of proposed roadway to be reclassified:

Cuba/Algonquin

7. Additional roadway jurisdiction(s), if any, of the proposed roadway to be reclassified:

NA

8. Current functional classification for this roadway:

Minor Arterial

9. Proposed functional classification for this roadway:

Major Collector

10. The IDOT [key route designation](#) number for this roadway: **9-3708**

(This number is available on the IDOT [Getting Around Illinois](#) website. The key route designation number is the Key Route Type, a hyphen, and the Key Route Number off the map.)

11. Endpoints of proposed roadway to be reclassified

- North or East endpoint: Plum Tree Road
- North or East endpoint road's functional classification:
Major Collector/Minor Arterial (Requested change to Major Collector)
- South or West endpoint: County Line Road
- South or West endpoint road's functional classification:
Minor Arterial



12. Length of proposed roadway to be reclassified:
2.0 Miles
13. Current Average Annual Daily Traffic (AADT):

3572 N. of County Line; 3620 S. of Plum Tree (2012 Hi-star counts by Village)

(Provide multiple AADTs by segment if the AADT is not consistent along the entire route. Indicate the source and year of the AADTs. Some AADTs are available on the IDOT [Getting Around Illinois](#) website. If the AADTs are not from a published source, supply raw field data and provide the date(s), the day(s) of week, the hours of collection, and the type of equipment used to collect the traffic data. HI-STAR or equivalent technology is preferred.)

14. Spacing:
- Provide the name of and distance to the next adjacent roadway (to the north or east) with the same classification as the subject road's proposed functional classification:
NA (Request for lower classification)
 - Provide the name of and distance to the next adjacent roadway (to the south or west) with the same classification as the subject road's proposed functional classification:
NA (Request for lower classification)

15. Indicate if you are proposing to change (downgrade) the functional classification of any adjacent roadways to accommodate the spacing requirements for your primary proposed functional classification revision:

NA

(Provide [key route designation](#) number and endpoints as well as road name and proposed change.)

16. Provide current and planned Traffic Signalization along proposed route:
(Mark locations on the map with a rectangle with three circles inside it, or similar; use the same symbol and write "future" by the planned signals.)
None
17. Provide current and planned Stop Sign Control on proposed route and on the cross-streets:
(Mark locations on the map with an octagon or similar; use the same symbols and write "future" by the planned signs.)

All-way stops at Plum Tree, Merri Oaks, and Spring Creek Road

18. Major Traffic Generators along the proposed reclassified route:

Residential subdivisions only.



19. Justification for the proposed revision based on definitions, characteristics and spacing guidance provided:

Existing character of service intended is to provide direct access to abutting land and discourage through traffic movements.

("To establish federal funding eligibility" is NOT a justification.)

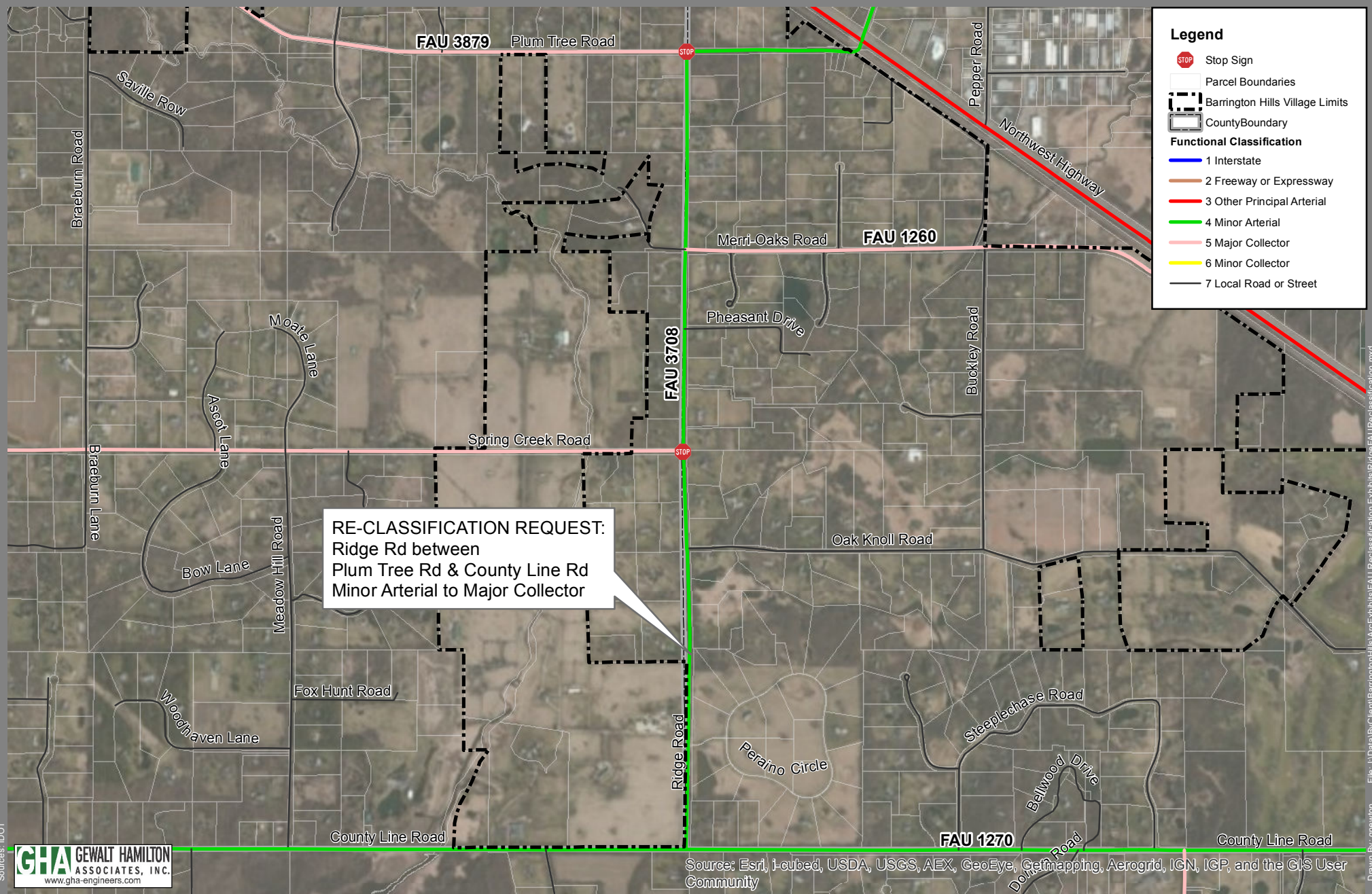
20. Provide any additional (optional) information or justification:

VBH Comprehensive Plan lists Ridge Road as "Local Service Road". IDOT response to request for "Local Street" classification noted Major Collector designation acceptable.

21. Attach Support Resolutions & Letters:

1. Local Council of Mayors or Councils of Mayors resolution(s) of support (required)
2. Affected neighboring jurisdictions' letters of support (required)
3. Requesting municipality's resolution of request (optional)





GHA GEWALT HAMILTON
ASSOCIATES, INC.
www.gha-engineers.com



1 inch = 2,300
Feet

IDOT Functional Re-Classification Request

Ridge Rd between Plum Tree Rd & County Line Rd
Village of Barrington Hills, IL

Appendix A

Functional Classification Revision Request Template

1. Name(s) of proposed roadway to be reclassified:

Plum Tree Road

2. Name of agency requesting revision (roadway jurisdiction):

Village of Barrington Hills

3. Contact information (name, title, address, phone and email):

Robert Kosin, Village Administrator, 112 Algonquin Rd, 847-551-3000, rkosin@barringtonhills-il.gov

4. Council(s) of Mayors:

McHenry County

5. County(ies) of proposed roadway to be reclassified:

Lake/McHenry

6. Township(s) of proposed roadway to be reclassified:

Cuba/Algonquin

7. Additional roadway jurisdiction(s), if any, of the proposed roadway to be reclassified:

NA

8. Current functional classification for this roadway:

Minor Arterial (east of Ridge), Major Collector (west of Ridge)

9. Proposed functional classification for this roadway:

Major Collector (Revision requested for section east of Ridge)

10. The IDOT [key route designation](#) number for this roadway: **9-3879**

(This number is available on the IDOT [Getting Around Illinois](#) website. The key route designation number is the Key Route Type, a hyphen, and the Key Route Number off the map.)

11. Endpoints of proposed roadway to be reclassified

- North or East endpoint: Northwest Highway (US 14)
- North or East endpoint road's functional classification:

Other Principal Arterial

- South or West endpoint: Ridge Road
- South or West endpoint road's functional classification:

Minor Arterial (Requested change to Major Collector)



12. Length of proposed roadway to be reclassified:
0.4 Miles
13. Current Average Annual Daily Traffic (AADT):

4960 E. of Ridge (2012 Hi-star counts by Village)

(Provide multiple AADTs by segment if the AADT is not consistent along the entire route. Indicate the source and year of the AADTs. Some AADTs are available on the IDOT [Getting Around Illinois](#) website. If the AADTs are not from a published source, supply raw field data and provide the date(s), the day(s) of week, the hours of collection, and the type of equipment used to collect the traffic data. HI-STAR or equivalent technology is preferred.)

14. Spacing:
- Provide the name of and distance to the next adjacent roadway (to the north or east) with the same classification as the subject road's proposed functional classification:
NA (Request for lower classification)
 - Provide the name of and distance to the next adjacent roadway (to the south or west) with the same classification as the subject road's proposed functional classification:
NA (Request for lower classification)

15. Indicate if you are proposing to change (downgrade) the functional classification of any adjacent roadways to accommodate the spacing requirements for your primary proposed functional classification revision:

NA

(Provide [key route designation](#) number and endpoints as well as road name and proposed change.)

16. Provide current and planned Traffic Signalization along proposed route:
(Mark locations on the map with a rectangle with three circles inside it, or similar; use the same symbol and write "future" by the planned signals.)
Traffic signal at US 14
17. Provide current and planned Stop Sign Control on proposed route and on the cross-streets:
(Mark locations on the map with an octagon or similar; use the same symbols and write "future" by the planned signs.)
Four-way stop at Ridge Road; stop control @ RR tracks/Cuba Road intersection.
18. Major Traffic Generators along the proposed reclassified route:

Residential subdivisions only.



19. Justification for the proposed revision based on definitions, characteristics and spacing guidance provided:

Existing character of service intended is to provide direct access to abutting land and discourage through traffic movements.

("To establish federal funding eligibility" is NOT a justification.)

20. Provide any additional (optional) information or justification:













VBH Comprehensive Plan lists Plum Tree Road as "Local Service Road".

21. Attach Support Resolutions & Letters:

1. Local Council of Mayors or Councils of Mayors resolution(s) of support (required)
2. Affected neighboring jurisdictions' letters of support (required)
3. Requesting municipality's resolution of request (optional)



Legend

-  Traffic Signal
-  Stop Sign
-  Parcel Boundaries
-  Barrington Hills Village Limits
-  County Boundary
- Functional Classification**
-  1 Interstate
-  2 Freeway or Expressway
-  3 Other Principal Arterial
-  4 Minor Arterial
-  5 Major Collector
-  6 Minor Collector
-  7 Local Road or Street



RE-CLASSIFICATION REQUEST:
Plum Tree Rd between
IL Rt. 14 & Ridge Rd
Minor Arterial to Major Collector

Source: Esri, i-cubed, USDA, USGS, AEX, GeoEye, Getmapping, Aerogrid, IGN, IGP, and the GIS User Community



1 inch = 600 Feet

IDOT Functional Re-Classification Request

Plum Tree Rd between IL Rt. 14 & Ridge Rd
Village of Barrington Hills, IL

Chapter 3

STOPPING, STANDING AND PARKING

9-3-1: APPLICABILITY:

In addition to the provisions of the Illinois Vehicle Code adopted by reference pursuant to Section [9-1-1](#), the following regulations shall apply. (Ord. 85-1, 1-28-85)

9-3-2: AUTHORITY TO REGULATE, PROHIBIT STOPPING, STANDING AND PARKING:

- (A) Authority to Regulate Stopping, Standing and Parking: The Roads and Bridges Committee of the Village Board is authorized, subject to approval by resolution of the Village Board of Trustees, to determine and designate those streets or parts of streets upon which stopping, standing or parking shall be prohibited within certain hours or permitted for a limited time, and it shall cause to be erected and maintained appropriate signs or curb markings giving notice thereof.
- (B) Authority to Prohibit Parking: The Roads and Bridges Committee of the Village Board is authorized, subject to approval by resolution of the Board of Trustees, to determine and designate those streets or parts of streets upon which parking shall be prohibited or upon which parking shall be prohibited on one side only, and it shall cause to be placed appropriate signs or curb markings giving notice thereof. (Ord. 85-1, 1-28-85)

9-3-3: AUTHORITY TO DESIGNATE FIRE LANES:

The Roads and Bridges Committee of the Village Board of Trustees, with the approval of the Village Board by resolution, may designate fire lanes and shall cause appropriate signs designating such fire lanes to be erected and maintained. (Ord. 85-1, 1-28-85)