

VILLAGE OF BARRINGTON HILLS

Roads & Bridges Committee

NOTICE OF MEETING



Thursday, May 19, 2016 ~ 4:00 pm
112 Algonquin Road

AGENDA

1. Call to Order & Roll Call
2. Public Comments
3. [Vote] Minutes April 21, 2016
4. Discussion Items
 - 4.1 2016 Road Program
 - a. Bid Review
 - b. Pavement Striping Program
 - c. Bridge Inspections
 - d. Highland Avenue/Spring Creek Road Project – Village of Algonquin
 - 4.2 Road Repairs - Cuba Township
 - 4.3 Drainage
 - a. Old Hart Road
 - b. Chapel Road
 - c. Lakeview Lane
 - d. Woodcreek Drainage Easement
 - 4.4 Cuba Road Bridge Update
 - 4.5 Seasonal Weight Limits- Lifted May 2, 2016
5. New Business
 - 5.1 Adopt A Road Program Interest
 - 5.2 Memorial Grove - Cuba Township
6. Adjournment

Chairman: Brian Cecola

NOTICE AS POSTED

VILLAGE OF BARRINGTON HILLS

Roads & Bridges Committee

Meeting Minutes April 21, 2016



Committee Members Present: Trustee Brian Cecola, Chair
Dan Strahan, Village Engineer
Robert Kosin, Director of Administration

Others Present: Ken Meek, Civil Engineer Linda H. Cools, Resident
Arnold Cernik, Resident

1. ORGANIZATIONAL: The meeting of the Village of Barrington Hills Roads & Bridges Committee was called to order at 4:00 p.m. by Chairman Cecola.

2. PUBLIC COMMENTS: Mr. Cernik asked why didn't the engineer plan to have the utilities moved or properly inspected in advance to avoid delays on Cuba Road Bridge? Mr. Strahan replied that a set of construction plans were supplied to Nicor, ComEd, and AT&T showing locations of the where work was taking place. The utility companies were responsible for relocating the utilities. The companies did move the utilities; however, when construction began, it was found that there was a conflict in place with the relocated utilities. The village as well as Gewalt Hamilton Associates do not supervise utility installation or relocation and assume no responsibility for this conflict. Arnold followed up to ask then if the contractor will get penalized for not meeting the deadline. Mr. Strahan answered that they cannot be penalized for delays that are outside of their control; however, if they fail to perform and do not meet the revised contract completion data than liquidated damages would apply.

3. APPROVAL OF MINUTES: The minutes of the Roads & Bridges Committee Meeting of March 17, 2016 were approved as written.

4.1 2016 ROAD PROGRAM UPDATE: Mr. Strahan reiterated that the 2016 road program will consist of both patching and resurfacing of 2.9 miles of village maintained roads. In addition to the resurfacing of roads, there are 13 culverts to be replaced. Improvements to Lakeview lane drainage are currently under review. The scope of road resurfacing consists of milling and resurfacing the pavement except where the road is in good shape, in which case, the surface would be applied directly over the existing pavement. Pulverization is required on Healy Road and Lakeview Lane due to poor road conditions. A submittal has been made to the Illinois Department of Transportation for permitting on 2 roads where the culvert replacement work will be within the state's right of way.

4.2 WOODCREEK DRAINAGE EASEMENT: Mr. Strahan explained that when Etters Woodcreek subdivision was created, there was a storm sewer put in at the south end of lot 11. This storm sewer runs from the Woodcreek Road right of way all the way back to Spring Creek. As Mr. Strahan reviewed the resident's elevation certificate for flood insurance adjustments, he was aware that fill had been placed in the floodplain without a required permit or compensatory storage. The property owner hired an engineer to make small pond adjustments as well as install a pipe that connects into the existing storm sewer. The property owner is concerned because there are 2-3 culverts that contribute surface water to enter his property. Mr. Strahan wants to consolidate the culverts into one area of runoff that can tie in to the existing storm sewer system and discharge into Spring Creek. In order to achieve this, it is recommended that a 15 foot drainage easement be approved. This would give the village the right to tie in and the responsibility to maintain within the easement provided. Mr. Kosin added that before Etters Sub division was built, the location acted as a primary branch of tributary area for Spring Creek. Mr. Kosin recommended that the board of trustees accept the easement and pointed out past success of a similar circumstance on Braeburn Lane and Spring Creek Road. Mr. Kosin also recommended that the village accept responsibility for assisting the owner in tying into the existing storm sewer to ensure that the system functions properly.

4.3 LAKEVIEW LANE DRAINAGE: Mr. Strahan met with 2 residents earlier in the week to discuss Lakeview Lane drainage improvements. Lakeview Lane watershed and drainage were reviewed. There is a culvert on IL Route 68 south of Heather Lake where the invert is below normal water level of Heather Lake indicating the flow is back pitched. Residents also made him aware that the outlet structure of Heather Lake may have been modified. Mr. Strahan wants to verify outlet structure changes or drainage characteristics. Potential options include regrading of IL Route 68 and a storm sewer system that runs down the south side of 47 Lakeview Lane. Due to uncertainty that the resident will include a private portion to the system to tie into the public portion and discharge into Heather Lake, as well as other uncertainties, Mr. Strahan recommends that the drainage portion of Lakeview Lane be removed from the road program. Trustee Cecola confirms with Mr. Strahan that the actual bid will not be delayed on the project. Mr. Strahan says that the village will keep the road improvements in the bid but will reserve the right to swap roads if the drainage issue is not rectified and the saturated base does not permit resurfacing. Mr. Kosin wants to put together list of alternate roads to ensure there is a proper back up plan in place.

4.4 CUBA ROAD BRIDGE UPDATE- CONSTRUCTION STATUS & PLAQUE: Mr. Strahan updated that the Contractor returned to the site Wednesday, April 13th. Overall, there was a utility delay of 12 weeks. Due to this, a contract extension is needed with IDOT. Also, budget for construction observation is running out. The original contract completion date was May 13th. After the delay, bridge opening is expected in late July with a best case scenario of late June. Trustee Cecola along with residents and trustees have decided to name the plaque "Veterans Crossing". He thanks residents for e-mailing him with suggestions and points out there will be an additional location on the property where a memorial can be placed. Contents of the memorial will be discussed after the bridge is reopened.

4.5 HIGHLAND AVENUE/SPRING CREEK ROAD PROJECT-VILLAGE OF ALGONQUIN: The village of Algonquin has designed plans for Highland Avenue and Spring Creek Road west of Haegers Bend Road. The project went out to bid and was in the March 4th, 2016 IDOT letting. The estimated cost was at \$3.2 Million; however, the low bid from Berger excavating came in at \$1.9 Million showing that prices are decreasing. Federal STP funding is capped at \$1.5 Million or 80% of the overall construction cost, whichever is smaller, which means the Village of Algonquin will have significantly lower costs for the project. The initial Village of Barrington Hills costs based on the IGA were around \$58,000. After scaling proportionally with the low bid, the price comes out to \$35,800. Mr. Strahan believes this would come down even lower due to the percent of local funding decreasing. There will be a preconstruction meeting in the near future at IDOT. Mr. Kosin suggests exploring the idea of using MFT funds for this debt obligation. Mr. Strahan made it clear that the Village would need to pass a resolution and that he would check allowance with IDOT after bid.

4.6 LAKE COUNTY DOT-US14/HART ROAD INTERSECTION IMPROVEMNTS UPDATE: Mr. Strahan recalled the proposed intersection improvements including and that preliminary plans have been provided by Lake County Department of Transportation. The Village of Barrington led Phase 1 of the design before handing the project off to LCDOT. LCDOT had inquired with the Village of Barrington Hills about future maintenance of the bike path and the Village declined. The Village of Barrington agreed to take over maintenance of the bike path to avoid removing it from the design. Due to widening of the roads with proposed improvements, land acquisition is necessary from adjacent properties. Mr. Strahan has spoken to property owners about this process and will provide technical assistance. Trustee Cecola wanted to ensure that there was a resolution passed or a document that exempts the Village of Barrington Hills from any liability with injuries, plowing, or other things of that nature. Mr. Kosin ensures they are exempt and will provide the documents.

4.7. HELM ROAD DRAINAGE: Mr. Kosin described the location as the western portion of Helm at the intersection of Helm Road and Sioux Avenue. This location is on the border of the Village of Barrington Hills and the Village of Carpentersville. There is a natural depression south of the pavement. This spring, a resident called to notify the Village that the water was up above the depression and on the pavement beyond the white striping. A contractor came out and removed debris from the storm sewer system leading to the depression and notified it was in poor condition. The water level did go down after debris removal. Mr. Strahan indicated that positive flow was located in the existing pipe; however, the next step is to coordinate a more permanent solution with the Village of Carpentersville in the form of a new outlet structure due to the wooded nature of the nearby landscape.

5. ADJOURNMENT: The meeting was adjourned at 4:39 PM.

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Daniel J. Strahan, P.E., CFM
Gewalt Hamilton Associates

Date: September 24, 2015

Re: Recommended 2016-2020 Road Program

In anticipation of the 2016 budget process it would be appropriate to resume discussions that had begun last spring regarding the Village's approach to a multi-year road program. Attached is a spreadsheet with a recommended five-year road program (2016-2020). This five year program is based on our recommended target of 3.2 miles of resurfacing each year based on an average surface life of 12 years.

While the Board cannot bind future Boards to a particular set of expenditures, if approved the attached document would serve as a planning tool in preparation of the annual road program from year to year.

Proposed 2016-2020 Road Program

Year	Preliminary Budget (2015 dollars)*	Adjusted Preliminary Budget
2016	\$945,000	\$992,000
3.18 Miles	<i>Woodcreek Road Rebecca Drive Helm Road Healy Road Tamarack Lane</i>	<i>Lakeview Lane Tricia Lane Spring Creek Road (Local Share)</i>
2017	\$1,013,000	\$1,117,000
3.47 Miles	<i>Chapel Road Church Road Haegers Bend Road</i>	<i>River Road (west of Haegers Bend) Hawthorne Lane VH Asphalt Parking Lots</i>
2018	\$1,031,000	\$1,193,000
3.27 Miles	<i>Hickory Lane Ridge Road (County Line to Merri Oaks)</i>	<i>Merri Oaks Road River/Algonquin Road</i>
2019	\$822,000	\$999,000
2.76 Miles	<i>Spring Lane Little Bend Road</i>	<i>Old Hart Road Springwood Lane</i>
2020	\$908,000	\$1,159,000
3.15 Miles	<i>Creekside Lane Butternut Road Hills & Dales Road</i>	<i>Honeycutt Road Cuba Road</i>
Total Expenditure (2015 dollars)- \$4.7 million		
2016-2020 Mileage Resurfaced- 15.83 Miles (3.17 Miles/Year)		

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: May 16, 2016

Re: 2016 Road Program
Bid Review

Bids for the 2016 Road Program were received on Thursday, May 12th, 2016. The following documents are attached and will be included in the board packet for the meeting on Monday, May 23, 2016:

- Road Program Summary Document
- Bid Recommendation Letter recommending award of the contract to Lorig Construction.
- Draft Board Resolution & Contact Form

Similar to other road resurfacing programs that we have observed this spring, asphalt prices were significantly lower this year than in past years. The unit prices provided for HMA binder and surface by Lorig Construction were each \$55.00/TN, compared with \$64.75/TN and \$68.85/TN, respectively, in 2015.

The total amount budgeted for Road Maintenance Contracts in 2016 is \$912,000.00. Adding the low bid submitted by Lorig Construction (\$746,234.79) and the budgeted amount for the Village's share of the Highland/Spring Creek Road project (\$35,800.00) would bring the total expenditure to \$782,034.79. Additional anticipated expenditures include the following:

- Addition of two culvert replacements on Helm Road (as discussed at the April R&B Committee meeting; estimated addition- \$15,000).
- Addition of storm sewer improvements at 11 Woodcreek Road (estimated addition- \$15,000; see agenda item 4.2)

At the April meeting, the Committee had suggested that we examine potential additions to the Road Program based on the low asphalt prices anticipated. A copy of the 2016-2020 Road Program is attached for reference. Based on location the most logical segment to move up would be the resurfacing of the asphalt parking lots at the Village Hall, currently scheduled to be completed in 2017. Our preliminary estimate for this resurfacing based on the unit prices provided is \$90,000.

May 16, 2016

625 Forest Edge Drive, Vernon Hills, IL 60061

TEL 847.478.9700 ■ FAX 847.478.9701

www.gha-engineers.com

Mr. Martin McLaughlin
President
Village of Barrington Hills
112 Algonquin Road
Barrington Hills, Illinois 60010

Re: 2016 Village Road Program
Bid Recommendation

Dear Mr. McLaughlin:

On Thursday, May 12, 2016 at 2:00 PM bids for the above referenced project were received, opened and read aloud at the Village Hall. Attached is an analysis of the 2016 Road Program and the bid tabulations and summary of the bids received. The 2016 Road Program includes culvert replacement, patching, and resurfacing. Location of work includes Helm Road (Algonquin Rd. to Village Limits), Healy Road (Dundee Rd. to Penny Rd.), Woodcreek Road (Dundee Rd. to Cul-de-sac), Tamarack Lane (Old Sutton Rd. to Terminus), Hawthorne Lane (Route 59 to Otis Rd.), Lakeview Lane (Dundee Rd. to Cul-de-sac), and Rebecca Drive (Old Sutton Rd. to Terminus).

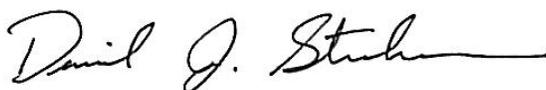
Five bids were received for this year's road program. The low bidder was Lorig Construction of Des Plaines, IL. Lorig Construction submitted a bid of \$746,234.79. Geske & Sons, Inc. of Crystal Lake, IL submitted the second lowest bid at \$756,872.60. The Engineer's Opinion of Probable Cost for this project was \$846,793.65. The amount in the 2016 budget for "Road Maintenance Contracts" is \$912,000.00.

While Lorig has not previously completed work in Barrington Hills, we have worked with them previously on roadway projects in other areas. Historically, they are a contracting firm involved in larger state and tollway projects but in the past few years have entered the municipal market. While their paving division is relatively new, they are a large and capable firm.

We recommend that the Village Board award the 2016 Road Program work package to Lorig Construction in the amount of \$746,234.79.

Lastly, as with any other construction project, it is recommended that the Village Board factor in a 10% contingency factor for additions or changes due to unforeseen conditions (generally below grade) to the proposed construction improvements. As always our office will keep the board apprised of any significant changes in the project scope. We anticipate the starting date for the 2016 Road Program would be mid- to late July. The contract documents specify a completion date of September 18, 2015.

Sincerely,
Gewalt Hamilton Associates, Inc.



Daniel J. Strahan, P.E., CFM
Village Engineer

cc: Brian Cecola, Road and Bridges Chair
Robert Kosin, Village Administrator

9355.147 Bid Recommendation- 2016 Road Program

MEMORANDUM

To: Board of Trustees
Village of Barrington Hills

625 Forest Edge Drive, Vernon Hills, IL 60061
TEL 847.478.9700 ■ FAX 847.478.9701

From: Daniel J. Strahan, P.E., CFM
Village Engineer

www.gha-engineers.com

Date: May 16, 2016

Re: 2016 Road Program Summary

Bids were received for the 2016 Road Program on Thursday, May 12th, 2016. The following pages provide a summary of the work to be completed as well as a review of the bids received.

2016 Road Program Summary

The goals of the Barrington Hills Road Program are to repair and maintain the pavement conditions of 39.0 miles of Village roadways to an acceptable condition, and to reduce maintenance costs to the Village over the life of the roadways. The 2016 Road Program includes culvert replacements, patching, and resurfacing. Location of work includes Helm Road (Algonquin Rd. to Village Limits), Healy Road (Dundee Rd. to Penny Rd.), Woodcreek Road (Dundee Rd. to Cul-de-sac), Tamarack Lane (Old Sutton Rd. to Terminus), Hawthorne Lane (Route 59 to Otis Rd.), Lakeview Lane (Dundee Rd. to Cul-de-sac), and patching on Rebecca Drive (Old Sutton Rd. to Terminus).

Helm Road

- Physical Conditions- Helm Road
 - 4,124 linear feet (0.78 miles) from Algonquin Road to Village limits
 - Existing Width Varies- Average 23'.
 - Limited platted right-of-way within project limits (See Fig. 1)

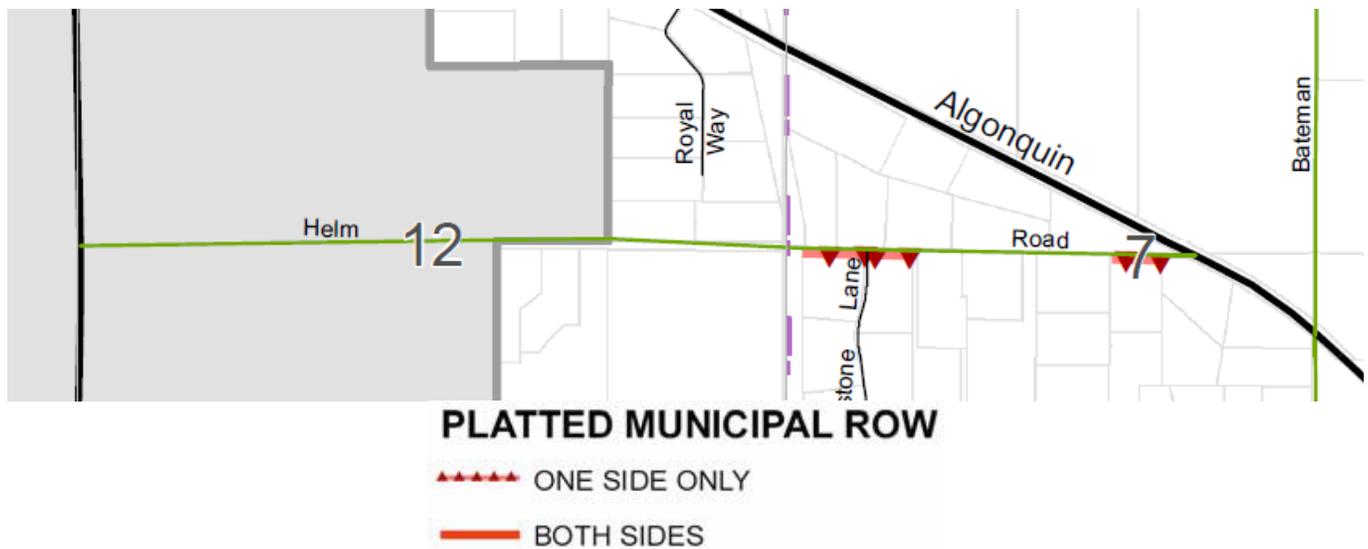


Fig. 1- Location of Platted ROW - Helm Road



Fig. 2- Helm Road facing east toward intersection with Algonquin Rd



Fig. 3- Helm Road and Algonquin Road intersection facing west



Fig. 4- Helm Road from village limits facing east

Healy Road

- Physical Conditions- Healy Road
 - 4,178 linear feet (0.79 miles) from Dundee Road to Penny Road.
 - Existing Width Varies - 18' average.
 - No platted right-of-way (prescriptive easement only)



Fig. 5 - Location of Platted ROW - Healy Road



Fig. 6 - Healy Road facing north toward intersection with IL-68 (Dundee Road)



Fig. 7 - Healy Road in between Penny Road and IL-68 (Dundee Road) facing North



Fig. 8 - Healy Road facing southwest towards Penny Road

Woodcreek Road

- Physical Conditions
 - Woodcreek Road- 2,422 linear feet (0.46 miles) from Dundee Road to cul-de-sac
 - Existing Width - 19' average
 - 66' Platted Right-of-way (See Fig. 9)



Fig. 9 - Location of Platted ROW- Woodcreek Road



Fig. 10 - Woodcreek Road facing north towards intersection with IL-68 (Dundee Road)



Fig. 11 - Woodcreek Road facing southeast



Fig. 12 - Woodcreek Road facing southeast towards cul-de-sac (terminus)

Tamarack Lane

- Physical Conditions
 - Tamarack Lane- 1,091 linear feet (0.21 miles) Old Sutton Road to Terminus
 - Existing Width - 14' average
 - Project Limits (See Figure)



Fig.13 - Location of Platted ROW- Tamarack Lane



Fig.14 - Tamarack Lane facing east toward intersection with Old Sutton Road



Fig.15 - Tamarack Lane intersection facing south



Fig.16 - Tamarack Lane intersection facing north

Hawthorne Lane

- Physical Conditions
 - Hawthorne Lane- 1,725 linear feet (0.33 miles) Route 59 to Otis Road
 - Existing Width - 16' average
 - Platted ROW throughout Project Limits

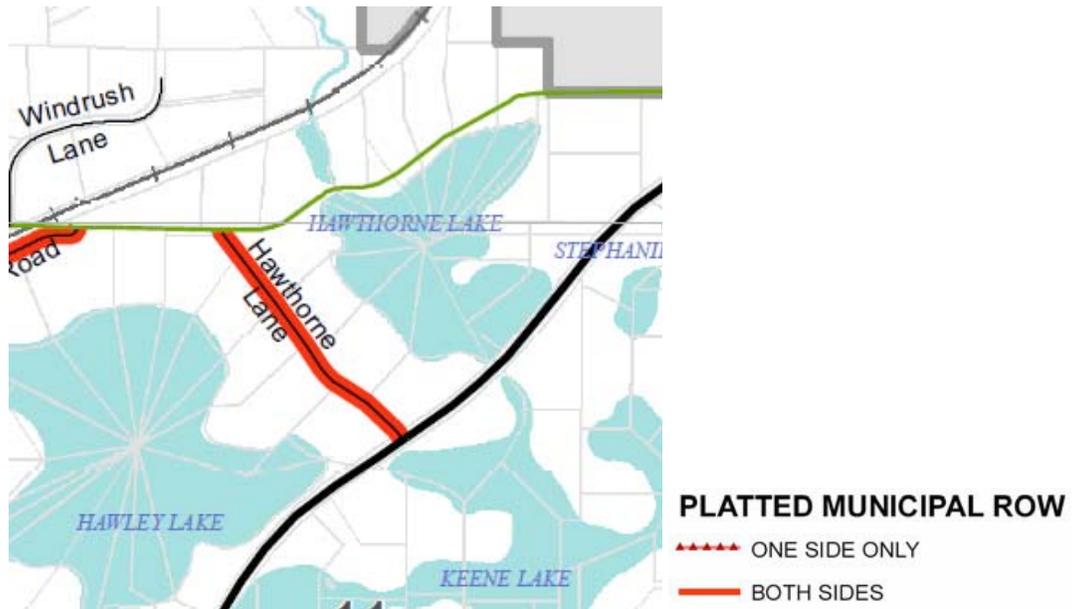


Fig. 17 - Location of Platted ROW- Hawthorne Lane



Fig.18 - Hawthorne Lane facing northwest toward Otis Road



Fig.19 - Hawthorne Lane between IL-59 (Hawthorne Road) and Otis Road facing northwest

Lakeview Lane

- Physical Conditions
 - Lakeview Lane- 1,909 linear feet (0.36 miles) Dundee Road to Cul-de-sac
 - Existing Width - 17' average
 - Platted ROW throughout Project Limits



Fig. 20 - Location of Project limits- Lakeview Lane



Fig. 21 – Facing south toward intersection of Lakeview Lane and IL-68 (Dundee Road)



Fig. 22 – From intersection of Lakeview Lane and IL-68 (Dundee Road) facing north

2016 Road Program- Bid Summary

Five bids for the 2016 Road Program were received on Thursday, May 12, 2016. Below is a summary of the bids received:

• Lorig Construction Company (Des Plaines, IL)	\$746,234.79 (\$48.30/FT)
• Geske & Sons, Inc. (Crystal Lake, IL)	\$756,872.60
• Arrow Road Construction (Mount Prospect, IL)	\$781,873.86
• Chicagoland Paving (Lake Zurich, IL)	\$877,900.00
• JA Johnson Paving (Arlington Heights, IL)	\$878,878.00
• <i>Engineer's Opinion of Probable Cost</i>	<i>\$846,793.65 (\$54.81/FT)</i>

The low bid submitted by Lorig Construction represents a significant 21.5% decrease over the low bid unit prices submitted last year. Below, the adjusted cost per linear foot of this year's project is compared to adjusted costs over the last several years:

• 2010 Resurfacing Cost:	\$39.58/LF
• 2011 Resurfacing Cost (Patching on other roads removed):	\$42.38/LF
• 2012 Resurfacing Cost (Caesar Storm & Steeplechase culverts removed):	\$43.87/LF
• 2013 Resurfacing Cost	\$41.90/LF
• 2014 Resurfacing Cost (Culvert Replacements removed):	\$59.11/LF
• 2015 Resurfacing Cost (Culvert Replacements removed):	\$56.28/LF
• 2016 Resurfacing Cost (Culvert Replacements removed):	\$44.16/LF

The largest item in any resurfacing program is the material cost of asphalt, which is driven in part by the price of oil. The primary reason for the reduction in costs from last year to this year is the reduction in the asphalt pricing provided by Lorig Construction. The unit prices provided for HMA binder and surface were each \$55.00/TN, compared with \$64.75/TN and \$68.85/TN, respectively, in 2015.

2016 Roads & Bridges Budget Summary

• Road Maintenance Contracts	<u>\$912,000.00</u>
Total Budgeted Amount	<u>\$912,000.00</u>

There are sufficient funds budgeted to complete the full scope of resurfacing included in the bid package, totaling 2.92 miles.

GHA Bid Recommendations

We recommend that the Village Board award the 2016 Road program work package in the amount of \$746,234.79 to Lorig Construction.

cc: Robert Kosin, Director of Administration

County Cook
Local Public Agency Barrington Hills
Section Number N/A
Route Various

1. THIS AGREEMENT, made and concluded the _____ day of _____, _____
Month and Year
between the Village of Barrington Hills
acting by and through its Board of Trustees known as the party of the first part, and
Lorig Construction Company his/their executors, administrators, successors or assigns,
known as the party of the second part.
2. Witnesseth: That for and in consideration of the payments and agreements mentioned in the Proposal hereto attached, to be made and performed by the party of the first part, and according to the terms expressed in the Bond referring to these presents, the party of the second part agrees with said party of the first part at his/their own proper cost and expense to do all the work, furnish all materials and all labor necessary to complete the work in accordance with the plans and specifications hereinafter described, and in full compliance with all of the terms of this agreement and the requirements of the Engineer under it.
3. And it is also understood and agreed that the LPA Formal Contract Proposal, Special Provisions, Affidavit of Illinois Business Office, Apprenticeship or Training Program Certification, and Contract Bond hereto attached, and the Plans for Section N/A, in the Village of Barrington Hills, approved by the Illinois Department of Transportation on _____, are essential documents of this contract and are a part hereof.
Date

4. IN WITNESS WHEREOF, The said parties have executed these presents on the date above mentioned.

Attest: _____ Clerk
The Village of Barrington Hills
By _____
Party of the First Part

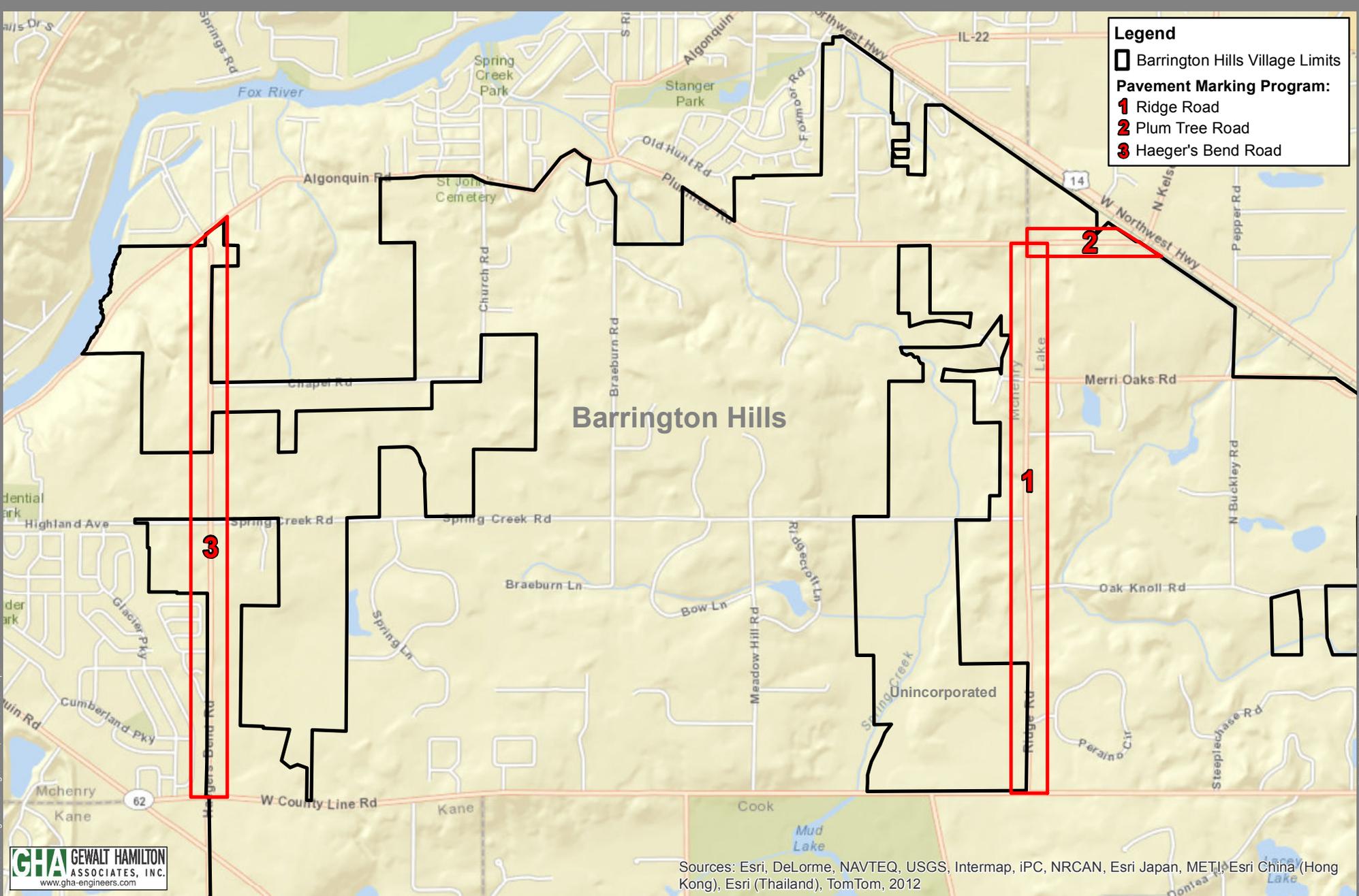
(Seal)
(If a Corporation)
Corporate Name _____
By _____
President Party of the Second Part
(If a Co-Partnership)

Attest: _____
Secretary

Partners doing Business under the firm name of

Party of the Second Part
(If an individual)

Party of the Second Part



Legend

- Barrington Hills Village Limits
- Pavement Marking Program:**
- 1** Ridge Road
- 2** Plum Tree Road
- 3** Haeger's Bend Road

Sources: Village of Barrington Hills, ESRI Street Maps



Sources: Esri, DeLorme, NAVTEQ, USGS, Intermap, iPC, NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri (Thailand), TomTom, 2012

File: G:\1903\3559355_134_2014_Road Program\GIS\ArcExhibits\Mapping\9355_134_PavementMarkingProgram.mxd

Drawn By: gnewton



1 inch = 2,500 Feet



2014 Pavement Marking Program

Barrington Hills, Illinois

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: May 16, 2016

Re: 2016 Bridge Inspections

The Village has four structures (the Cuba Road Bridge, the Green Rail Bridge on Oak Knoll Road, the Porter Bridge on Oak Knoll Road, and the Spring Creek culverts under Spring Creek Road) that are included in the National Bridge Inventory System. As a result, each of these bridges require bi-annual inspections by a structural engineer be submitted to IDOT. Wiss, Janney, Elstner Associates, Inc. is the Bridge Program Manager and completes these bi-annual inspections.

Bi-annual inspections were last completed in 2014 so will be required again in 2016. The expense for these inspections is included in the "Bridge Inspections" line item under the Roads & Bridges fund within the approved 2016 budget. WJE will be completing these inspections later this summer, to include an initial "inventory inspection" of the Cuba Road Bridge structure once completed, as required by IDOT.

MEMORANDUM

To: Brian Cecola, VBH Chairman Roads & Bridges
Robert Kosin, VBH Director of Administration

From: Daniel J. Strahan, P.E., CFM
Gewalt Hamilton Associates

Date: May 9, 2016

Re: Village of Algonquin
Highland Avenue/Spring Creek Road Project

On Tuesday, May 3rd, a preconstruction meeting took place at the Illinois Department of Transportation Schaumburg headquarters. Members from Berger Excavating (the project contractor), the Village of Algonquin, Christopher B. Burke Engineering, and IDOT attended. The scope of the project consists of full depth HMA removal, curb and gutter, sewers, detention basins and outfall restrictors, multi-use HMA path, sidewalks, pedestrian crossings, warning lights, markings, landscaping, and resurfacing on Highland Ave. from Tanglewood Dr. to Haegers Bend Rd. in Algonquin. As of right now, the start date is undetermined. The intent was for Berger to mobilize and begin work within the next week or two (as of May 3rd); however, there are both utility and road closure delays.

The utility delays are with ComEd, Nicor, and AT&T. ComEd has 4 poles to relocate and hopes to be finished by May 31st barring emergency work or weather. Nicor has estimated a May 26th completion date as long as weather permits. NPL has installed approximately 2200' of the 4160' as of May 5th. AT&T has put together draft permitting for the relocation plan. On Monday, May 9th, a joint utility meeting took place at Algonquin public works to discuss the delays and proper course of action. Members from AT&T and ComEd attended. As of May 9th, ComEd is still planning for a May 31st completion date. AT&T anticipates a 3 week bid process and an additional 3-4 weeks for the relocation work. They expect to finish by late June but will do everything they can to expedite the process.

The Village of Algonquin has mandated maintaining park access up until July 15th for little league baseball games and therefore cannot provide full road closure up until that date. This was not listed in the specifications and was a surprise to Berger; ideally they would not like to begin work until all utility work is complete and the closure can begin. Depending on how close to the July 15th date the utility work is complete, they may just hold off until full closure is in place before work begins.

The Village of Algonquin has said they will come up with a notification to send out to Barrington Hills residents before the project begins. Angelo Milazzo has said that the village can give input or make recommendations as to the location of patches within village limits. Initial construction was planned for 90 working days which would have put the completion date around mid-October. There is obvious concern to expedite this process and finish through binder before paving season terminates so the road is open for winter. There are other means and methods such as completing the road all at once, instead of in phases, which may cut down on construction time for the project. More project scheduling will be discussed once the utility work is complete. In the meantime, Berger is submitting their construction schedule (per original contract specifications) to IDOT so that they can request the appropriate time extension due to utility delays and also all paperwork so that when the time comes they will be ready to begin work. We will update the village as more information becomes available.



Dan Strahan <dstrahan@gha-engineers.com>

Fwd: Drainage at 41 Old Hart Road

Dan Strahan <dstrahan@gha-engineers.com>
To: Mary Beth Richards <richardsmb@msn.com>
Cc: Robert Kosin <rkosin@barringtonhills-il.gov>

Fri, May 13, 2016 at 1:13 PM

Mary Beth,

It was a pleasure meeting you this morning. As discussed, there is a significant consistent surface flow of water that originates west of 40 Old Hart Road, runs east to the ditchline along the west side of Old Hart Road, turns and runs approximately 200' south to the roadway culvert under Old Hart Road, and runs north through your property 200' before turning east to flow into Flint Creek. I think both you and the Village would benefit from a new culvert approximately 200' north of the existing roadway culvert, as illustrated on the attached exhibit. Both the creek west of Old Hart and the east-west portion of the swale are evident in the aerial photograph and are well aligned. A culvert at this location would significantly reduce the flow in the south-north portion of the swale which has become a maintenance burden to you.

Typically culvert improvements are completed concurrently with roadway resurfacing operations. Based on the current 5-year Road Program, Old Hart Road is anticipated to be resurfaced in 2019. Since it is a few years out, I will raise the topic at this months Roads & Bridges Committee meeting to discuss the possibility of completing the work sooner.

Dan

On Wed, May 11, 2016 at 1:26 PM, Mary Beth Richards <richardsmb@msn.com> wrote:

Thank you Bob! As I stated before, I think the new culvert on the property across the street has caused an increase in water drainage on our property. I will wait to hear from Dan Strahan regarding a property visit.

Thanks again,
Mary Beth Richards

> From: rkosin@barringtonhills-il.gov
> Date: Wed, 11 May 2016 12:58:34 -0500
> Subject: Fwd: Drainage at 41 Old Hart Road
> To: richardsmb@msn.com
> CC: dstrahan@gha-engineers.com

>

> Ms. Richards,

>

> As we discussed with the break in the weather by copy to the Village
> Engineer Dan Strahan, locations are being gather to examine for
> surface water flow.

>

> He will contact you directly with his field schedule as well as share
> with you the drainage records he may have for your area of the
> Village.

>

> - R. Kosin

>

>

> ----- Forwarded message -----

> From: Mary Beth Richards <richardsmb@msn.com>
> Date: Thu, May 5, 2016 at 9:44 PM
> Subject: Drainage at 41 Old Hart Road
> To: "rkosin@barringtonhills-il.gov" <rkosin@barringtonhills-il.gov>
>
>
> Bob,
>
> Beth and Dean Meyer are my neighbors on the west side of Old Hart
> Road. I told them about my drainage problem and that someone from the
> Village would be stopping by to look at the area. They have give
> their permission for Village personnel to enter their property to view
> the water drainage that flows from their property under Old Hart Road
> and onto our property.
>
> Today, Beth walked me around their property to show me their runoff
> areas, which were definitely flowing at a pretty good rate. Three
> years ago they built a barn to house their lawn equipment and had to
> extend their paved driveway to the barn, which crosses over their
> drainage area. They built a culvert under the new driveway to allow
> the water to drain. This was the same time we started to see an
> increase in water on our property.
>
> I would like to be present when the Village representative comes to
> view the area. I can be reached at [847-606-3176](tel:847-606-3176) to schedule the
> appointment.
>
> Thank you again for your time this morning.
>
> Mary Beth Richards
>
>
>
>
> --
> Robert Kosin
> Village of Barrington Hills
> 112 Algonquin Rd, Barrington Hills, IL 60010-5199
> [847.551.3000](tel:847.551.3000) | BarringtonHills-il.gov
>
> --
>
>
> To ensure compliance with the Open Meetings Act, elected or appointed
> members of the public body may reply to this message, but they should not
> forward it or send a copy of the reply to other members of the public body.

--

Dan Strahan, P.E., CFM
Associate/Senior Engineer
Gewalt Hamilton Associates, Inc.
625 Forest Edge Drive
Vernon Hills, IL 60061
[\(847\)-478-9700](tel:847-478-9700) (Main)
[\(847\)-821-6233](tel:847-821-6233) (Direct)

 **41 Old Hart Drainage Exhibit.pdf**
1006K

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Geomapping, AeroGRID, IGN, IGP, swisstopo, and the GIS User Community



Approximate Location of Proposed Culvert



1 inch = 100 Feet

41 Old Hart Road Drainage

Village of Barrington Hills

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

625 Forest Edge Drive, Vernon Hills, IL 60061

TEL 847.478.9700 ■ FAX 847.478.9701

www.gha-engineers.com

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: May 16, 2016

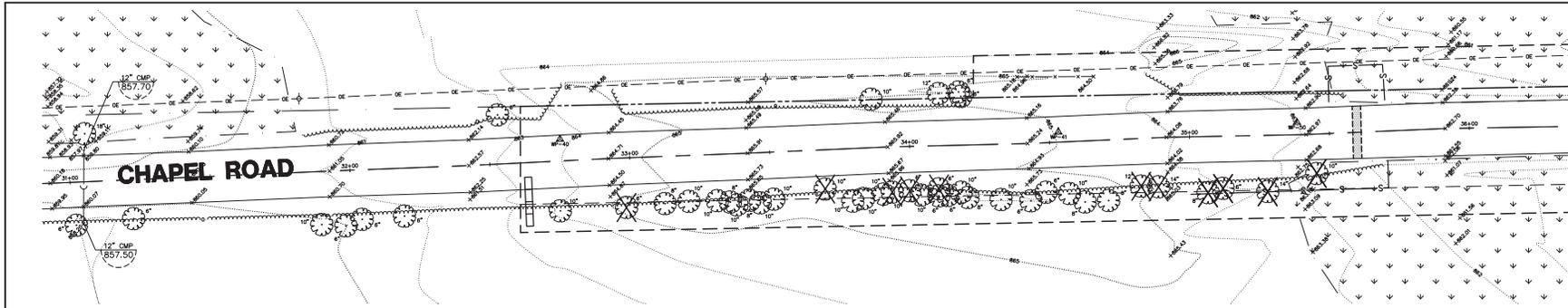
Re: Chapel Road Drainage

In 2012 storm sewer and folding “Water on Pavement” signage was installed near the east end of Chapel Road. This section of Chapel Road is located between two wetlands and has historically had poor drainage and as a result poor pavement conditions. During the permitting process, McHenry County Planning, Building, & Development wanted to ensure that the storm sewer would not drain down the wetlands, so they would not permit an invert elevation below the 100-year high water level. The project went forward as this would provide some relief from the persistent standing water elevations at the time which were 12-18” above the pavement elevation; the highest observed water elevation 863.65 in 2009.



March 2009 Flooding Condition on Chapel Road

Recently the “Water on Pavement” signs were deployed as water encroached onto the pavement. The approximate water elevation currently is 862.5, right at the rim elevation of the catch basin on the north side of Chapel Road. As the attached drawing shows, the system design, allows water above the 10-year HWL (862.5) to flow west during high water periods to two dry well catch basins. However, water does not flow further west to the downstream catch basin until it exceeds the calculated 100-year HWL of 863.2.

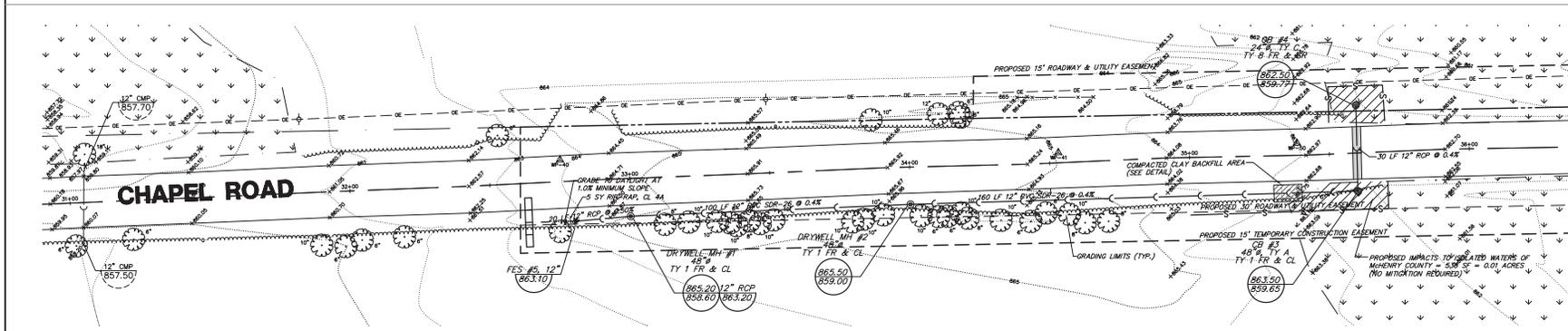


EXISTING/DEMO LEGEND

- SAWCUT PAVEMENT (FULL DEPTH)
- PAVEMENT REMOVAL (FULL DEPTH)
- EXISTING WETLAND AREA
- TREE REMOVAL

DEMOLITION NOTES:

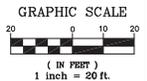
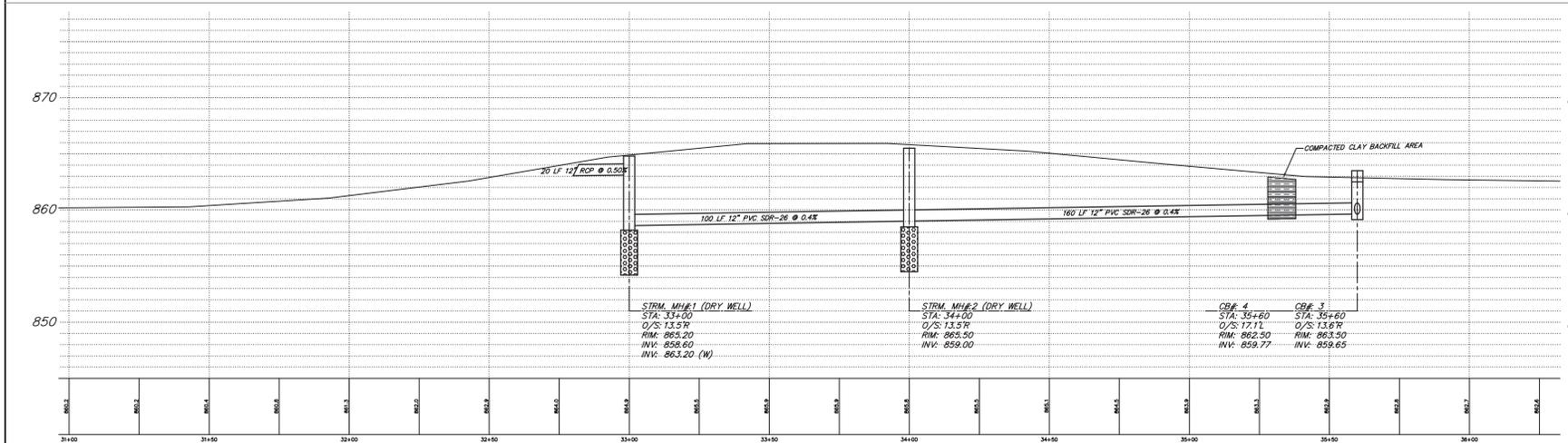
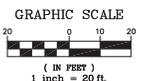
1. AS AN INITIAL ORDER OF OPERATION TRAFFIC CONTROL SIGNS MUST BE IN PLACE PRIOR TO ANY CONSTRUCTION OPERATIONS. A MINIMUM OF ONE LANE OF TRAFFIC MUST REMAIN OPEN AT ALL TIMES. IMPROVE FLAGGING. REORDER CONSTRUCTION OPERATIONS LANT TRAFFIC FLOW TO ONE LANE.
2. INLET FILTER BASKETS SHALL BE PLACED IN ALL DRAINAGE STRUCTURES PRIOR TO INITIATION OF CONSTRUCTION ACTIVITIES AND REMAIN IN PLACE THROUGHOUT CONSTRUCTION.
3. SAWCUT PAVEMENT (FULL DEPTH) 1" FROM EDGE OF EXISTING PAVEMENT (WHERE NOTED) AS MARKED BY ENGINEER.
4. PROVIDE BARRICADES ALONG EDGES OF EXCAVATION AT CLOSE OF EACH CONSTRUCTION DATE.
5. THE WETLAND BUFFER AREA SHALL BE STAYED IN THE FIELD PRIOR TO CONSTRUCTION AND PROTECTED FROM DEVELOPMENT IMPACTS DURING CONSTRUCTION.



PROPOSED LEGEND

- CLASS D PATCHING, 4"
- PROPOSED WETLAND IMPACT
- GRADING & SHAPING DITCHES

1. RESTORE ALL DISTURBED GRASS AREAS WITH 4" TOPSOIL, CLASS 2A SEEDING, & MFG S-75 EROSION CONTROL BLANKET (STAPLED IN PLACE), OR ALTERNATE SEED AND BLANKET AS NOTED ON SHEET 7.
2. ALL PROPOSED DRAINAGE SWALES SHALL BE BUILT TO MINIMUM 1:0.5 SLOPE. MINIMUM CROSS SLOPE OF DRAINAGE SWALES SHALL BE 3:1 V. PROPOSED DRIVEWAY SLOPES SHALL NOT EXCEED 10%.



GEWALT HAMILTON ASSOCIATES, INC.

CONSULTING ENGINEERS & SURVEYORS
850 Forest Edge Drive Vernon Hills, IL 60061 Tel. 847.478.9700 Fax 847.478.9701

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PLAN & PROFILE
CHAPEL ROAD DRAINAGE IMPROVEMENTS
2012 DRAINAGE PROGRAM
VILLAGE OF BARRINGTON HILLS, IL

5	D/S	8-24-11	FINAL PERMIT & BID SET				
4	D/S	6-30-11	REVISED PER MCHENRY COUNTY COMMENTS				
3	D/S	5-18-11	REVISED PER MCHENRY COUNTY COMMENTS				
2	D/S	2-14-11	REVISED PER MCHENRY COUNTY COMMENTS	7	D/S	2-7-12	FINAL PERMIT & BID SET - REDUCED SCOPE
1	D/S	12-1-10	PERMIT SUBMITTAL SET	6	D/S	12-19-11	PERMIT SET - REDUCED SCOPE
NO.	BY	DATE	REVISION	NO.	BY	DATE	REVISION

FILE	9355-107-PR6.dwg	SHEET NUMBER	3
DRAWN BY	JDJ	CHA PROJECT #	9355.107
DATE	06/14/09	CHECKED BY	VB
		DATE	06/19/09
		SCALE	1"=20'
			OF 5 SHEETS

MEMORANDUM

625 Forest Edge Drive, Vernon Hills, IL 60061

TEL 847.478.9700 ■ FAX 847.478.9701

www.gha-engineers.com

To: Robert Kosin, VBH Director of Administration
Brain Cecola, VBH Chairman Roads & Bridges

From: Daniel J. Strahan, P.E., CFM
Gewalt Hamilton Associates

Date: May 16, 2016

Re: Woodcreek Road- Proposed Drainage Easement

Following approval by the Village Board for a 15' drainage easement along the south end of 11 Woodcreek Road, Bob Kosin and I met with the property owners, David and Marla Russo, on Tuesday, May 10, 2016 at the Village Hall. Discussion items included the proposed culvert replacement within the Woodcreek Road right-of-way, extension of an 8" PVC pipe on within the Russo Property, and dedication of the easement. The following summarizes the outcome of the meeting:

- The Village will provide a copy of the proposed drainage easement along the south side of the property for review.
- The Village will be responsible for construction of the 8" PVC pipe on the Russo property. A Right-of-Entry permit will be drafted to allow the Village's contractor to access the property for review.
- Upon approval of the site drainage plan, the Village will sign off on the Russo's Elevation Certificate as required by FEMA.

A copy of the right-of-entry permit is enclosed for reference. As 8" PVC storm sewer was not included in the bid quantities for the 2016 Road Program, a change order will be requested from the contractor. We anticipate the additional costs from this work to be less than \$15,000.

Etters Woodcreek Estates Subdivision- Lot 11

**Right-of-Entry Permit
Village of Barrington Hills**

Property Owner: David & Marla Russo
PIN #: 01-20-201-006
Address: 11 Woodcreek Road (Etters Woodcreek Estates Subdivision, Lot 11)
Barrington Hills, Illinois 60045

It is to the mutual benefit of the Village of Barrington Hills and the undersigned that the Village of Barrington Hills be given the right to enter upon the property of the undersigned for the purpose of constructing a storm sewer.

Now, therefore, the undersigned, being owner of real estate located at, or identified in the County tax records as PIN #: 01-20-201-006, grants to Village of Barrington Hills, State of Illinois, for its use and use of others designated by it, the right and privilege to enter upon a portion of said real estate on a temporary basis for the purposes of constructing a stormsewer. The right to enter shall be limited to the existing driveway and adjacent lawn areas as is reasonably necessary to construct the proposed storm sewer.

All expenses of restoring the area after construction are to be assumed by the Village of Barrington Hills. It is also agreed that the premises will be restored and left in a neat and presentable condition. Any trees removed by the Village of Barrington Hills will not require tree replacement by the owner.

The Village of Barrington Hills shall indemnify and hold harmless the Owner and Owner's successors and assigns, from and against any and all claims, losses, damages, injuries, liability, costs and expenses, including reasonable attorneys' fees, arising from any accident, occurrence, injury, loss or damage to any person arising out of or resulting from the Village's use of the property except to the extent of the negligence or willful misconduct of the Owner.

This permit shall expire upon completion of the maintenance work including restoration and final acceptance, no later than October 31, 2016.

ACCEPTED BY THE OWNER(S):
This ____ Day of _____, 2016

ACCEPTED BY VILLAGE OF BARRINGTON HILLS
This ____ Day of _____, 2016

(Signature of Owner)
Print Name: _____

By: _____
Its _____

(Signature of Owner)
Print Name: _____

Attest: _____
Village Clerk

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: May 16, 2016

Re: Cuba Road Bridge Replacement Project
Status Update

The following is a summary of the current status of the Cuba Road Bridge project.

Work Progress

As discussed at the previous Committee meeting, on Wednesday, April 13, Copenhaver Construction remobilized to the site and has resumed construction operations after extensive utility delays. Since that time the west footings and south wingwalls have been constructed.

The next step in the construction process is for placement of the precast bridge structure. Copenhaver is awaiting final approval by IDOT of the shop drawings for the precast units, at which time their supplier will cast them and deliver them to the site. Placement of the precast units is anticipated the week of May 23.

Local Agency Agreement

To date, utility delays have resulted in 86 calendar days of downtime for Copenhaver Construction. AS a result, the contract completion date is required to be changed from May 13, 2016 to August 7, 2016. Additional engineering and construction costs are anticipated due to the utility delays incurred to date. Attached is an amendment to the Local Agency Agreement for the project as well as an amended Construction Services Agreement, which IDOT will require the Village to approve by resolution. It is recommended that the resolution be considered for approval at the May 23, 2016 Board meeting.

 Illinois Department of Transportation Local Public Agency Amendment # 1 for Federal Participation	Local Public Agency Village of Barrington Hills	State Contract <input checked="" type="checkbox"/>	Day Labor <input type="checkbox"/>	Local Contract <input type="checkbox"/>	RR Force Account <input type="checkbox"/>
	Section: 12-00020-00-BR	Fund Type: STP-Br	ITEP / SRTS / HSIP Number(s)		
Construction		Engineering		Right-of-Way	
Job Number	Project Number	Job Number	Project Number	Job Number	Project Number
C-91-083-13	BRM-4003(107)				

This Amendment is made and entered into between the above local public agency, hereinafter referred to as the "LPA", and the state of Illinois, acting by and through its Department of Transportation, hereinafter referred to as "STATE". The STATE and LPA have jointly proposed to improve the designated location as described below and agree to the changes outlined in this Amendment. The improvement shall be constructed in accordance with plans approved by the STATE and the STATE's policies and procedures approved and/or required by the Federal Highway Administration, hereinafter referred to as "FHWA".

Location

Location Cuba Rd Route FAU 1260 Length 0.10
Termini South of US 14 over Flint Creek

Current Jurisdiction Village of Barrington Hills TIP Number 11-12-0021 Existing Structure No 049-

Amended Division of Cost

Type of Work	STP-BR	%	%	LPA	%	Total
Participating Construction	946,000	(*)	()	236,000	(BAL)	1,182,000
Non-Participating Construction		()	()		()	
Preliminary Engineering		()	()		()	
Construction Engineering	140,688	(*)	()	35,172	(BAL)	175,860
Right of Way		()	()		()	
Railroads		()	()		()	
Utilities		()	()		()	
Materials						
TOTAL	\$ 1,086,688			\$ 271,172		\$ 1,357,860

*Maximum FHWA (STP-Br) participation 90% not to exceed \$1,086,688.

NOTE:

The costs shown in the Division of Cost table are approximate and subject to change. The final LPA share is dependent on the final Federal and State participation. The actual costs will be used in the final division of cost for billing and reimbursement.

If funding is not a percentage of the total, place an asterisk in the space provided for the percentage and explain above.

Local Public Agency Appropriation

For Amendments Increasing the LPA share: By execution of this Amendment, the LPA attests that additional moneys have been appropriated or reserved by resolution or ordinance to fund the additional share of LPA project costs. A copy of the resolution or ordinance is attached as an addendum (required for increases to state-let contracts only).

Local Agency Village of Barrington Hills	LOCAL AGENCY  Illinois Department of Transportation Construction Engineering Services Agreement For Federal Participation Supplement #1	CONSULTANT	Consultant Gewalt Hamilton Associates, Inc.
County Lake			Address 625 Forest Edge Drive
Section 12-00020-00-BR			City Vernon Hills
Project No. BRM-4003(107)			State IL
Job No. C-91-083-13			Zip Code 60061
Contact Name/Phone/E-mail Address Robert Kosin/ 847-551-300 rkosin@barringtonhills-il.gov		Contact Name/Phone/E-mail Address Dan Strahan/847-478-9700 dstrahan@gha-engineers.com	

THIS AGREEMENT is made and entered into this _____ day of _____, _____ between the above Local Agency (LA) and Consultant (ENGINEER) and covers certain professional engineering services in connection with the PROJECT described herein. Federal-aid funds allotted to the LA by the state of Illinois under the general supervision of the Illinois Department of Transportation (STATE) will be used entirely or in part to finance engineering services as described under AGREEMENT PROVISIONS.

WHEREVER IN THIS AGREEMENT or attached exhibits the following terms are used, they shall be interpreted to mean:

Regional Engineer	Deputy Director Division of Highways, Regional Engineer, Department of Transportation
Resident Construction Supervisor	Authorized representative of the LA in immediate charge of the engineering details of the PROJECT
In Responsible Charge	A full time LA employee authorized to administer inherently governmental PROJECT activities
Contractor	Company or Companies to which the construction contract was awarded

Project Description

Name Cuba Road Route FAU 1260 Length 0.10Mile Structure No. 049-6049

Termini Over Flint Creek

Description: Phase III Engineering for the replacement of the bridge carrying Cuba Road over Flint Creek

Agreement Provisions

I. THE ENGINEER AGREES,

1. To perform or be responsible for the performance of the engineering services for the LA, in connection with the PROJECT hereinbefore described and checked below:
 - a. Proportion concrete according to applicable STATE Bureau of Materials and Physical Research (BMPR) Quality Control/Quality Assurance (QC/QA) training documents or contract requirements and obtain samples and perform testing as noted below.
 - b. Proportion hot mix asphalt according to applicable STATE BMPR QC/QA training documents and obtain samples and perform testing as noted below.
 - c. For soils, to obtain samples and perform testing as noted below.
 - d. For aggregates, to obtain samples and perform testing as noted below.

NOTE: For 1a. through 1d. the ENGINEER is to obtain samples for testing according to the STATE BMPR "Project Procedures Guide", or as indicated in the specifications, or as attached herein by the LA; test according to the STATE BMPR "Manual of Test Procedures for Materials", submit STATE BMPR inspection reports; and verify compliance with contract specifications.

- e. Inspection of all materials when inspection is not provided at the sources by the STATE BMPR, and submit inspection reports to the LA and the STATE in accordance with the STATE BMPR "Project Procedures Guide" and the policies of the STATE.
 - f. For Quality Assurance services, provide personnel who have completed the appropriate STATE BMPR QC/QA trained technician classes.
 - g. Inspect, document and inform the LA employee In Responsible Charge of the adequacy of the establishment and maintenance of the traffic control.
 - h. Geometric control including all construction staking and construction layouts.
 - i. Quality control of the construction work in progress and the enforcement of the contract provisions in accordance with the STATE Construction Manual.
 - j. Measurement and computation of pay items.
 - k. Maintain a daily record of the contractor's activities throughout construction including sufficient information to permit verification of the nature and cost of changes in plans and authorized extra work.
 - l. Preparation and submission to the LA by the required form and number of copies, all partial and final payment estimates, change orders, records, documentation and reports required by the LA and the STATE.
 - m. Revision of contract drawings to reflect as built conditions.
 - n. Act as resident construction supervisor and coordinate with the LA employee In Responsible Charge.
2. Engineering services shall include all equipment, instruments, supplies, transportation and personnel required to perform the duties of the ENGINEER in connection with the AGREEMENT.
 3. To furnish the services as required herein within twenty-four hours of notification by the LA employee In Responsible Charge.
 4. To attend meetings and visit the site of the work at any reasonable time when requested to do so by representatives of the LA or STATE.
 5. That none of the services to be furnished by the ENGINEER shall be sublet, assigned or transferred to any other party or parties without the written consent of the LA. The consent to sublet, assign or otherwise transfer any portion of the services to be furnished by the ENGINEER shall not be construed to relieve the ENGINEER of any responsibility for the fulfillment of this AGREEMENT.
 6. The ENGINEER shall submit invoices, based on the ENGINEER's progress reports, to the LA employee In Responsible Charge, no more than once a month for partial payment on account for the ENGINEER's work completed to date. Such invoices shall represent the value, to the LA of the partially completed work, based on the sum of the actual costs incurred, plus a percentage (equal to the percentage of the construction engineering completed) of the fixed fee for the fully completed work.
 7. That the ENGINEER is qualified technically and is entirely conversant with the design standards and policies applicable to improvement of the SECTION; and that the ENGINEER has sufficient properly trained, organized and experienced personnel to perform the services enumerated herein.
 8. That the ENGINEER shall be responsible for the accuracy of the ENGINEER's work and correction of any errors, omissions or ambiguities due to the ENGINEER'S negligence which may occur either during prosecution or after acceptance by the LA. Should any damage to persons or property result from the ENGINEER's error, omission or negligent act, the ENGINEER shall indemnify the LA, the STATE and their employees from all accrued claims or liability and assume all restitution and repair costs arising from such negligence. The ENGINEER shall give immediate attention to any remedial changes so there will be minimal delay to the contractor and prepare such data as necessary to effectuate corrections, in consultation with and without further compensation from the LA.
 9. That the ENGINEER will comply with applicable federal statutes, state of Illinois statutes, and local laws or ordinances of the LA.
 10. The undersigned certifies neither the ENGINEER nor I have:
 - a) employed or retained for commission, percentage, brokerage, contingent fee or other considerations, any firm or person (other than a bona fide employee working solely for me or the above ENGINEER) to solicit or secure this AGREEMENT;

- b) agreed, as an express or implied condition for obtaining this AGREEMENT, to employ or retain the services of any firm or person in connection with carrying out the AGREEMENT or
 - c) paid, or agreed to pay any firm, organization or person (other than a bona fide employee working solely for me or the above ENGINEER) any fee, contribution, donation or consideration of any kind for, or in connection with, procuring or carrying out the AGREEMENT.
 - d) are not presently debarred, suspended, proposed for debarment, declared ineligible or voluntarily excluded from covered transactions by any Federal department or agency;
 - e) have not within a three-year period preceding the AGREEMENT been convicted of or had a civil judgment rendered against them for commission of fraud or criminal offense in connection with obtaining, attempting to obtain or performing a public (Federal, State or local) transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements or receiving stolen property;
 - f) are not presently indicted for or otherwise criminally or civilly charged by a government entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph (e) of this certification; and
 - g) have not within a three-year period preceding this AGREEMENT had one or more public transactions (Federal, State or local) terminated for cause or default.
11. To pay its subconsultants for satisfactory performance no later than 30 days from receipt of each payment from the LA.
 12. To submit all invoices to the LA within one year of the completion of the work called for in this AGREEMENT or any subsequent Amendment or Supplement.
 13. To submit BLR 05613, Engineering Payment Report, to the STATE upon completion of the work called for in the AGREEMENT.
 14. To be prequalified with the STATE in Construction Inspection when the ENGINEER or the ENGINEER's assigned staff is named as resident construction supervisor. The onsite resident construction supervisor shall have a valid Documentation of Contract Quantities certification.
 15. Will provide, as required, project inspectors that have a valid Documentation of Contract Quantities certification.

II. THE LA AGREES,

1. To furnish a full time LA employee to be In Responsible Charge authorized to administer inherently governmental PROJECT activities.
2. To furnish the necessary plans and specifications.
3. To notify the ENGINEER at least 24 hours in advance of the need for personnel or services.
4. To pay the ENGINEER as compensation for all services rendered in accordance with this AGREEMENT, on the basis of the following compensation formulas:

Cost Plus Fixed Fee Formulas

- $FF = 14.5\%[DL + R(DL) + OH(DL) + IHDC]$, or
- $FF = 14.5\%[(2.3 + R)DL + IHDC]$

Where: DL = Direct Labor
 IHDC = In House Direct Costs
 OH = Consultant Firm's Actual Overhead Factor
 R = Complexity Factor
 FF=Fixed Fee
 SBO = Services by Others

Total Compensation = DL +IHDC+OH+FF+SBO

- Specific Rate (Pay per element)
- Lump Sum _____

5. To pay the ENGINEER using one of the following methods as required by 49 CFR part 26 and 605 ILCS 5/5-409:

With Retainage

- a) **For the first 50% of completed work**, and upon receipt of monthly invoices from the ENGINEER and the approval thereof by the LA, monthly payments for the work performed shall be due and payable to the ENGINEER, such payments to be equal to 90% of the value of the partially completed work minus all previous partial payments made to the ENGINEER.
- b) **After 50% of the work is completed**, and upon receipt of monthly invoices from the ENGINEER and the approval thereof by the LA, monthly payments covering work performed shall be due and payable to the ENGINEER, such payments to be equal to 95% of the value of the partially completed work minus all previous partial payments made to the ENGINEER.
- c) **Final Payment** – Upon approval of the work by the LA but not later than 60 days after the work is completed and reports have been made and accepted by the LA and the STATE, a sum of money equal to the basic fee as determined in this AGREEMENT less the total of the amounts of partial payments previously paid to the ENGINEER shall be due and payable to the ENGINEER.

Without Retainage

- a) **For progressive payments** – Upon receipt of monthly invoices from the ENGINEER and the approval thereof by the LA, monthly payments for the work performed shall be due and payable to the ENGINEER, such payments to be equal to the value of the partially completed work minus all previous partial payments made to the ENGINEER.
- b) **Final Payment** – Upon approval of the work by the LA but not later than 60 days after the work is completed and reports have been made and accepted by the LA and STATE, a sum of money equal to the basic fee as determined in this AGREEMENT less the total of the amounts of partial payments previously paid to the ENGINEER shall be due and payable to the ENGINEER.

6. The recipient shall not discriminate on the basis on the basis of race, color, national origin or sex in the award and performance of any DOT-assisted contract or in the administration of its DBE program or the requirements of 49 CFR part 26. The recipient shall take all necessary and reasonable steps under 49 CFR part 26 to ensure nondiscrimination in the award and administration of DOT-assisted contracts. The recipient's DBE program, as required by 49 CFR part 26 and as approved by DOT, is incorporated by reference in this agreement. Implementation of this program is a legal obligation and failure to carry out its terms shall be treated as violation of this agreement. Upon notification to the recipient of its failure to carry out its approved program, the Department may impose sanctions as provided for under part 26 and may, in appropriate cases, refer the matter for enforcement under 18 U.S.C. 1001 and/or the Program Fraud Civil Remedies Act of 1986 (31U.S.C. 3801 et seq.).

7. To submit approved form BC 775 (Exhibit C) and BC 776 (Exhibit D) with this AGREEMENT.

III. It is Mutually Agreed,

- 1. That the ENGINEER and the ENGINEER's subcontractors will maintain all books, documents, papers, accounting records and other evidence pertaining to cost incurred and to make such materials available at their respective offices at all reasonable times during the AGREEMENT period and for three years from the date of final payment under this AGREEMENT, for inspection by the STATE, Federal Highway Administration or any authorized representatives of the federal government and copies thereof shall be furnished if requested.
- 2. That all services are to be furnished as required by construction progress and as determined by the LA employee In Responsible Charge. The ENGINEER shall complete all services specified herein within a time considered reasonable to the LA, after the CONTRACTOR has completed the construction contract.
- 3. That all field notes, test records and reports shall be turned over to and become the property of the LA and that during the performance of the engineering services herein provided for, the ENGINEER shall be responsible for any loss or damage to the documents herein enumerated while they are in the ENGINEER's possession and any such loss or damage shall be restored at the ENGINEER's expense.
- 4. That this AGREEMENT may be terminated by the LA upon written notice to the ENGINEER, at the ENGINEER's last known address, with the understanding that should the AGREEMENT be terminated by the LA, the ENGINEER shall be paid for any services completed and any services partially completed. The percentage of the total services which have been rendered by the ENGINEER shall be mutually agreed by the parties hereto. The fixed fee stipulated in numbered paragraph 4d of Section II shall be multiplied by this percentage and added to the ENGINEER's actual costs to obtain the earned value of work performed. All field notes, test records and reports completed or partially completed at the time of termination shall become the property of, and be delivered to, the LA.
- 5. That any differences between the ENGINEER and the LA concerning the interpretation of the provisions of this AGREEMENT shall be referred to a committee of disinterested parties consisting of one member appointed by the ENGINEER, one member appointed by the LA, and a third member appointed by the two other members for disposition and that the committee's decision shall be final.
- 6. That in the event the engineering and inspection services to be furnished and performed by the LA (including personnel furnished by the ENGINEER) shall, in the opinion of the STATE be incompetent or inadequate, the STATE shall have the right to supplement the engineering and inspection force or to replace the engineers or inspectors employed on such work at the expense of the LA.

7. That the ENGINEER has not been retained or compensated to provide design and construction review services relating to the contractor's safety precautions, except as provided in numbered paragraph 1f of Section I.
8. This certification is required by the Drug Free Workplace Act (30ILCS 580). The Drug Free Workplace Act requires that no grantee or contractor shall receive a grant or be considered for the purpose of being awarded a contract for the procurement of any property or service from the State unless that grantee or contractor will provide a drug free workplace. False certification or violation of the certification may result in sanctions including, but not limited to, suspension of contract or grant payments, termination of a contract or grant and debarment of contracting or grant opportunities with the State for at least one (1) year but no more than five (5) years.

For the purpose of this certification, "grantee" or "contractor" means a corporation, partnership or other entity with twenty-five (25) or more employees at the time of issuing the grant, or a department, division or other unit thereof, directly responsible for the specific performance under a contract or grant of \$5,000 or more from the State, as defined in the Act.

The contractor/grantee certifies and agrees that it will provide a drug free workplace by:

- (a) Publishing a statement:
 - (1) Notifying employees that the unlawful manufacture, distribution, dispensing, possession or use of a controlled substance, including cannabis, is prohibited in the grantee's or contractor's workplace.
 - (2) Specifying the actions that will be taken against employees for violations of such prohibition.
 - (3) Notifying the employee that, as a condition of employment on such contract or grant, the employee will:
 - (A) abide by the terms of the statement; and
 - (B) notify the employer of any criminal drug statute conviction for a violation occurring in the workplace no later than five (5) days after such conviction.
 - (b) Establishing a drug free awareness program to inform employees about:
 - (1) the dangers of drug abuse in the workplace;
 - (2) the grantee's or contractor's policy of maintaining a drug free workplace;
 - (3) any available drug counseling, rehabilitation and employee assistance program; and
 - (4) the penalties that may be imposed upon an employee for drug violations.
 - (c) Providing a copy of the statement required by subparagraph (a) to each employee engaged in the performance of the contract or grant and to post the statement in a prominent place in the workplace.
 - (d) Notifying the contracting or granting agency within ten (10) days after receiving notice under part (B) of paragraph (3) of subsection (a) above from an employee or otherwise receiving actual notice of such conviction.
 - (e) Imposing a sanction on, or requiring the satisfactory participation in a drug abuse assistance or rehabilitation program by, any employee who is convicted, as required by section S of the Drug Free Workplace Act.
 - (f) Assisting employees in selecting a course of action in the event drug counseling, treatment and rehabilitation is required and indicating that a trained referral team is in place.
 - (g) Making a good faith effort to continue to maintain a drug free workplace through implementation of the Drug Free Workplace Act.
9. The ENGINEER or subconsultant shall not discriminate on the basis of race, color, national origin or sex in the performance of this AGREEMENT. The ENGINEER shall carry out applicable requirements of 49 CFR part 26 in the administration of DOT-assisted contracts. Failure by the ENGINEER to carry out these requirements is a material breach of this AGREEMENT, which may result in the termination this AGREEMENT or such other remedy as the LA deems appropriate.

Agreement Summary

Prime Consultant:	TIN Number	Agreement Amount
Gewalt Hamilton Associates	36-3426053	\$40,477.13
Sub-Consultants:	TIN Number	Agreement Amount
Sub-Consultant Total:		\$0.00
Prime Consultant Total:		\$40,477.13
Total for all Work:		\$40,477.13

Executed by the LA:

Village of Barrington Hills

(Municipality/Township/County)

ATTEST:

By: _____
 _____ Clerk

By: _____
 Title: _____

(SEAL)

Executed by the ENGINEER:

ATTEST:

By: _____

By: _____

Title: _____

Title: _____

AVERAGE HOURLY PROJECT RATES

FIRM Gewalt Hamilton Associates
PTB _____
PRIME/SUPPLEMENT Prime

DATE 05/02/16

SHEET 1 OF 5

PAYROLL CLASSIFICATION	AVG HOURLY RATES	TOTAL PROJECT RATES			Project Coordination & Docum			Construction Observation & I			Hours	% Part.	Wgtd Avg	Hours	% Part.	Wgtd Avg	Hours	% Part.	Wgtd Avg
		Hours	% Part.	Wgtd Avg	Hours	% Part.	Wgtd Avg	Hours	% Part.	Wgtd Avg									
Principal Engineer	64.08	0																	
Senior Engineer	49.65	20	5.00%	2.48	10	11.11%	5.52	10	3.23%	1.60									
Sr. Environmental Consultant	40.00	0																	
Professional Engineer	35.25	0																	
Registered Land Surveyor	43.63	0																	
Staff Engineer	24.50	0																	
Environmental Consultant	26.00	0																	
Sr. Engineering Tech	32.19	380	95.00%	30.58	80	88.89%	28.61	300	96.77%	31.15									
Engineering Tech II	24.50	0																	
Engineering Tech I	16.82	0																	
Engineer Tech Intern	14.13	0																	
Clerical/Administrative	23.88	0																	
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TOTALS		400	100%	\$33.06	90	100%	\$34.13	310	100%	\$32.75	0	0%	\$0.00	0	0%	\$0.00	0	0%	\$0.00

April 26, 2016

Mr. Martin McLaughlin
Village President
Village of Barrington Hills
112 Algonquin Road
Barrington Hills, Illinois 60010

Re: 2016 Road Posting
Village of Barrington Hills

Dear Mr. McLaughlin:

Based on our recent field observations of the roadway conditions on Village-maintained roads and consideration of extended weather forecasts, our office recommends that the seasonal load restrictions be lifted for all Village roads on Monday, **May 2, 2016**.

It is our opinion that adequate pavement strength has returned to most of the roadways from the spring thaw with reduced moisture conditions. With the expectations of continued warm temperatures the structural integrity of the road base will continue to increase and better support heavier loads. Considering these factors it is appropriate the road posting restrictions be lifted.

Sincerely,
Gewalt Hamilton Associates, Inc.



Daniel J. Strahan, P.E., CFM
Village Engineer

cc: Brian Cecola, Roads and Bridges Chair
Robert Kosin, Village Administrator
Anna Paul, Village Clerk